

SUBDIVISION REGULATIONS

YAZOO CITY, MISSISSIPPI

AN ORDINANCE PROVIDING REGULATIONS GOVERNING SUBDIVISION OF LAND WITHIN THE CORPORATE LIMITS OF THE CITY OF YAZOO CITY, MISSISSIPPI: PROVIDING THE PROCEDURE FOR THE ADMINISTRATION THEREOF; ESTABLISHING PLAT REQUIREMENTS, REQUIRED IMPROVEMENTS, AND DESIGN STANDARDS; PROVIDING FOR VARIANCES; AND FOR RELATED PURPOSES:

WHEREAS, the Mayor and Board of Aldermen of the City of Yazoo City, Mississippi, acting under authority granted by Section 2890.5 and Sections 3590 through 3597, Volume 3 Recompiled, Mississippi Code of 1942, as amended, have taken action heretofore to provide for the preparation, adoption, amendment, extension, and implementation of an official comprehensive plan, in whole or in part, for the purpose of bringing about coordinated physical development of the City of Yazoo City, in accordance with present and future needs; and have created and established heretofore a City Planning Commission, to serve without pay, with authority to prepare and propose a comprehensive plan of physical development of the City of Yazoo City, Mississippi, or part thereof; a proposed zoning ordinance and map; regulations governing the subdivision of land; and building or setback lines on roads and highways; and

WHEREAS, the City Planning Commission has prepared and proposed to the Board of Aldermen of the City of Yazoo City a master plan of physical development for the City of Yazoo City, Mississippi, or part thereof; a proposed zoning ordinance and map; regulations governing the subdivision of land; and building and setback lines along all streets and highways; and

WHEREAS, the Mayor and Board of Aldermen of the City of Yazoo City, after a public hearing called and held thereon as provided by Section 3593, Volume 3 Recompiled, Mississippi Code of 1942, as amended, have adopted heretofore an official comprehensive plan of physical development for the City of Yazoo City, Mississippi, that is based upon the proposals submitted to the Board of Aldermen by the City Planning Commission; and

WHEREAS, the City Planning Commission subsequently has submitted and proposed to the Board of Aldermen for adoption regulations governing subdivision of land within the corporate limits of the City of Yazoo City, Mississippi, which regulations are based upon and in accordance with the official comprehensive plan of physical development adopted by the Board of Aldermen heretofore; and

9/1/72

WHEREAS, the Board of Aldermen has called and held a public hearing on the proposed regulations governing subdivision of land within the corporate limits of the City of Yazoo City in the manner provided by Section 3593, Volume 3 Recompiled, Mississippi Code of 1942, as amended, at which public hearing parties in interest and citizens were given the opportunity to be heard and were so heard; and

WHEREAS, the regulations governing subdivision of land within the corporate limits of the City of Yazoo City, Mississippi, set forth in this ordinance are based upon the official comprehensive plan of physical development of the City of Yazoo City adopted by the Board of Aldermen heretofore; and are designed to assist the orderly, efficient, and coordinated development within the City of Yazoo City; to promote the health, safety, and general welfare of the residents of Yazoo City and its environs; to facilitate the adequate provision of transportation, water, sewerage, schools, parks, and other public improvements; and to secure equitable handling of all subdivision plats by providing uniform procedures and standards for observance by both subdividers and the City Planning Commission;

NOW, THEREFORE, be it ordained by the Mayor and Board of Aldermen that these regulations shall govern all subdivision of land within the corporate limits of the City of Yazoo City, Mississippi, after the date of their adoption.

John McCre
Building Inspector
ARTICLE I

PURPOSE, AUTHORITY, JURISDICTION, TITLE, AND DEFINITIONS

A. Purpose

Land subdivision is the first step in the process of community development. Once land has been cut up into streets, lots, and blocks, and has been publicly recorded, correction of defects is costly and difficult. Subdivision of land sooner or later becomes a public responsibility, because roads and streets must be maintained and public services customary to urban areas must be provided. Since the welfare of the entire community is thereby affected in many important respects, it is therefore in the best interests of the public, the developer, and the future owners that subdivisions be designed and developed according to adequate minimum standards.

The following subdivision standards are designed to provide for the harmonious development of the land area of the community; to secure a coordinated lot layout and facilitate the movement of traffic; and to provide adequate light, air, water, sewer facilities, as well as recreation, transportation, and communication facilities.

B. Authority

These subdivision regulations are adopted pursuant to the authority set forth in Title 13, Chapter 1, Section 2890.5; and Title 16, Article VII, Section 3374-123 and 123.5, of the Mississippi Code of 1942 as amended.

C. Jurisdiction

These regulations shall apply to the land area located within the Yazoo City corporate limits.

These regulations are designed, intended, and should be administered so as to:

1. implement the community's comprehensive plan;
2. assist neighborhood conservation, and prevent the development of blight and decay;
3. develop the various tracts of land in harmony with the existing community, and facilitate the future development of any adjoining unsubdivided tracts of land;

4. provide that the cost of improvements which primarily benefit the tract of land being subdivided be borne by the owners or developers of the tract, and that the cost of improvements which primarily benefit the whole community be borne by the whole community;
5. promote the best possible design and layout for each tract of land being subdivided; and
6. establish adequate, accurate records of all land subdivision taking place within the Yazoo City corporate limits.

D. Title

These regulations shall be known as the "Subdivision Regulations, City of Yazoo City, Mississippi," and may be so cited.

E. Definitions

For the purposes of this resolution, the following words, terms, phrases, and their derivations shall have the meanings given herein. When not inconsistent with the context, words used in the present tense shall include the future, words used in the singular number shall include the plural number, and the word "shall" shall be interpreted as mandatory, not merely directory.

ACCESSORY BUILDING: A subordinate building located on the same lot with the main building, but the use of which is incidental to that of the main building.

ACCESSORY USE: A use customarily incidental, appropriate, and subordinate to the principal use of land or buildings, and located on the same lot therewith.

ALLEY: A minor right-of-way dedicated to public use which provides a secondary means of vehicular access to the back or side of properties otherwise abutting a street. This right-of-way may often be used for public utility purposes.

BLOCK: A parcel of land intended to be used for urban purposes which is entirely surrounded by public streets, highways, railroad rights-of-way, public walks, parks or greenstrips, rural land or drainage channels, or a combination thereof.

BOARD OF ADJUSTMENT: Whenever used in this ordinance, shall mean the Yazoo City Board of Adjustment, appointed by the Mayor and Board of Aldermen to hear and decide any appeals to the requirements spelled out by this ordinance.

BUILDABLE AREA OF A LOT: That portion of a lot bounded by the front setback required line and the side and rear building setback lines upon which a structure may be placed.

BUILDING INSPECTOR: Whenever used in this ordinance, shall refer to the person or his authorized representative appointed by the Mayor and Board of Aldermen to enforce the provisions of this ordinance.

BUILDING, MAIN, OR PRINCIPAL: A building in which is conducted the principal use of the lot on which it is situated. A dwelling shall be deemed to be the main building on the lot on which it is situated in all residential districts.

BUILDING SETBACK LINE: A line delineating the minimum allowable distance between the street right-of-way and the front of a structure within which no building or other structure shall be placed. (The building setback line is parallel to or concentric with the street right-of-way line.)

CITY: Whenever used in this ordinance, shall mean the City of Yazoo City, Mississippi.

COMMUNITY FACILITIES PLAN: The part of the Yazoo City Comprehensive Plan now or hereafter adopted which shows the locations of existing and proposed schools, parks, recreational sites, fire and police stations, libraries, and so forth.

COMPREHENSIVE PLAN (or General Plan): The Comprehensive (Development) Plan for the City of Yazoo City which has been officially adopted to provide long-range development policies for the area subject to urbanization in Yazoo City, Mississippi.

CUL-DE-SAC (Court or Dead End Street): A short street having one end open to traffic, and permanently terminated by a vehicle turnaround.

DEAD END STREET: Any local street, other than a cul-de-sac, which has only one outlet.

DEVELOPER: Any person engaged in developing or improving a lot or group of lots, or placing structures thereon for use or occupancy.

DEVELOPMENT: The act of building structures or installing site improvements.

DWELLING: A building or portion thereof, exclusive of mobile homes as herein defined, used for residential purposes.

EASEMENT: A grant by the property owner of the use of a strip of land by the public, a corporation, or persons, for specified purposes.

FILL: The placing, storing, or dumping of any material such as earth, clay, sand, concrete, rubble, or waste of any kind upon the surface of the ground which results in increasing the natural surface elevation.

FLOODWAY: The channel of a river or stream, and those parts of the floodplain adjoining the channel, which are reasonable required to carry and discharge the flood flow of the various creeks.

FLOODWAY FRINGE AREA: The relatively low area or flatlands adjoining the channel of a river, stream, watercourse, ocean, lake, or other body of standing water which has been or may be covered by floodwater.

FRONTAGE: All the property on one side of a street between two (2) intersecting streets (crossing or terminating), measured along the line of the street, or if the street is dead ended, then all of the property abutting on one side between an intersecting street and the dead end of the street.

GOVERNING BODY: Whenever used in the text of this ordinance, shall mean the Mayor and Board of Aldermen of the City of Yazoo City, Mississippi.

GRADE, FINISHED: The completed surfaces of lawns, walks, and roads, as shown on official plans or designs relating hereto.

HEALTH DEPARTMENT: Whenever used in this ordinance, shall mean the Yazoo County Health Department.

LOT: A parcel of land which is or may be occupied by a building and its accessory buildings and uses customarily incident thereto, together with such yards or open spaces within the lot lines as may be required by this ordinance, and fronting upon a publicly dedicated street.

LOT AREA: The total horizontal area included within the lot lines.

LOT, CORNER: A lot of which at least two (2) adjoining sides abut for their full lengths on public streets.

LOT DEPTH: The average distance from the street right-of-way line to the rear lot line, measured along the side lines of the lot.

LOT, DOUBLE FRONTAGE: A lot which runs through a block from street to street, having two (2) nonintersecting sides abutting on two (2) or more streets.

LOT FRONTAGE: That measurement of a lot abutting on a public street, taken along the street right-of-way line from side lot line to side lot line. (Structures on corner lots fronting on a particular street shall be deemed to have frontage on that street.)

LOT, INTERIOR: A lot other than a corner lot.

LOT LINE~~s~~: The line~~s~~ bounding a lot as defined herein.

LOT LINE, FRONT: In the case of an interior lot, the line separating said lot from the street. In the case of a corner or double frontage lot, the line separating said lot from the street which the house will face, to be determined from the request for a Building Permit.

LOT LINE, REAR: The lot boundary opposite and most distant from the front lot line. In the case of a pointed or irregular lot, it shall be an imaginary line parallel to and farthest from the front lot line.

LOT LINE, SIDE: The property boundary line connecting the front and rear lot lines.

LOT WIDTH: The width of a lot at the front building setback line, measured parallel to the street right-of-way line.

MAJOR THOROUGHFARE (or Primary Street): A street designed for large volumes of traffic. Major thoroughfares may be classified by the comprehensive plan into several types.

MARGINAL ACCESS STREET: A local street parallel with and adjacent to a major thoroughfare which provides access to abutting properties and protection from through traffic.

OFFICIAL MAP: That part of the comprehensive plan now or hereafter adopted, which includes a major street, highway, park, and parkway plan, and which sets forth the identification, location, dimensions, and classification of existing and proposed public streets, highways, parkways, and park and recreation sites.

OWNER: Any person having a sufficient proprietary interest in the land sought to be subdivided to commence and maintain proceedings to subdivide the same according to the provisions specified in this ordinance.

PEDESTRIAN WAY: A right-of-way, however designated, either across or within a block, intended for use by pedestrian traffic.

PERSON: A corporation, firm, partnership, trust, association, or any other legal entity, as well as a natural person.

PLANNING COMMISSION: Whenever used in this ordinance, shall mean the Planning Commission of the City of Yazoo City, Mississippi.

PLAT: A map, plan, or layout indicating the location and boundaries of individual properties.

PLAT, FINAL: A map of a land subdivision prepared in suitable form for filing, including all necessary affidavits, dedications, and acceptances; complete bearings and dimensions of all lines defining lots and blocks, streets and alleys, and public areas; and all other dimensions.

PLAT, PRELIMINARY: A map of a proposed land subdivision showing the character and proposed layout of the tract in sufficient detail to indicate the suitability of the land proposed to be subdivided.

PRINCIPAL USE: The specific primary purpose for which land or a building is intended to be used.

PRIVATE DRIVE: A right-of-way which has the characteristics of a street as defined herein, except that it has not been dedicated for public use. A driveway located on a lot which serves only that lot is not considered a private drive.

PUBLIC USES: Whenever used in this ordinance, shall refer to public parks, schools, hospitals, and administrative, cultural, and service buildings.

PUBLIC UTILITY: Any person, firm, corporation, municipal department, or board duly authorized under state or municipal regulations to furnish such public services as electricity, gas, water, sewer, telephone, telegraph, transportation, or other services to its subscribers or customers.

QUARRY: A lot or parcel of land or part thereof used for the purpose of extracting stone, sand, gravel, or soil for sale.

RECREATIONAL FACILITIES: Country clubs, riding stables, golf courses, swimming pools, playgrounds, recreation centers, and other noncommercial recreation areas and facilities.

SANITARY SEWER: A public or community sewage disposal system of a type approved by the Air and Water Pollution Control Commission, or individual sewage disposal systems of a type approved by the State Department of Public Health.

STORY: That portion of a building included between the surface of any floor and the surface of the floor next above it; if there be no floor above it, then the space between the floor and the ceiling next above it.

STORY, HALF: A story under a gable, hip, or gambrel roof, the wall plates of which on at least two (2) opposite exterior walls are not more than two (2) feet above the floor of such story.

STREET: A right-of-way other than an alley, dedicated or otherwise legally established for public use, and usually affording the principal means of access to abutting property. A street may be designated as a highway, thoroughfare, parkway, boulevard, road, avenue, lane, drive, or other appropriate name.

STREET, COLLECTOR: A street designed to facilitate the collection of traffic from local streets, to provide circulation within neighborhood areas, and to provide a convenient way for traffic to reach the primary and secondary streets.

STREET GRADE: The established grade of the street upon which a lot fronts. If there is no established grade, the grade of the street at the center of the right-of-way shall be considered as the street grade.

STREET, INTERSECTING: Any street which joins another street at an angle whether or not it crosses the other.

STREET, LIMITED ACCESS: Major thoroughfares to which ingress and egress are provided only at controlled intersections, and which deny access to abutting properties.

STREET, ~~LOCAL~~ OR MINOR: A street designed primarily to provide access to abutting properties, usually residential. Some minor streets may be marginal access streets parallel to limited access primary and/or secondary streets, and providing access from properties abutting such marginal access streets to access points along such limited access streets.

STREET, PRIMARY: A street designed to move large volumes of traffic. Primary streets are usually classified as limited access streets, to which entrances and exits are provided only at controlled intersections; they also deny access to abutting properties.

STREET RIGHT-OF-WAY LINE: The legal property boundary line delineating the street right-of-way and the abutting property.

STREET, SECONDARY: A street designed to facilitate the collection of traffic from feeder streets, and usually located on neighborhood boundaries. Secondary streets are usually classified as limited access streets, to which entrances and exits are provided only at controlled intersections; they also deny access to abutting properties.

STRUCTURE: Anything constructed or erected, the use of which requires permanent location on the ground or attachment to something having a permanent location on the ground, including advertising signs, billboards, and poster panels.

SUBDIVIDER: Any person, firm, partnership, corporation, or other entity acting as a unit, and subdividing or proposing to subdivide land as herein defined.

SUBDIVISION: The division or redivision of land into two or more lots, tracts, sites, or parcels for the purpose of transfer of ownership, or for development.

SURVEYOR: A land surveyor registered by the State of Mississippi.

USABLE OPEN SPACE: That portion of a lot which is free of buildings, is not devoted to driveway or parking areas, is available and accessible to all the occupants of dwelling units on the lot, and is large enough to be used for active or passive recreation or other outdoor activities. Usable open space may include playlots, gardens, sundecks, courts, courtyards, and other required yard areas.

USE: The specific purpose for which land or a building is designed, arranged, or intended, or for which it is or may be occupied or maintained. The term "permitted use" or its equivalent shall not be deemed to include any nonconforming use.

WAY: A street, thoroughfare, or easement permanently established for passage of persons or vehicles.

YARD: An unoccupied space between a building and the adjoining lot lines which remain unobstructed by any structure from the ground upward.

YARD, FRONT: An unoccupied space on the same lot with the principal building, extending the full width of the lot, and situated between the street right-of-way line and the front building setback line. The depth of the front yard shall be measured between the front building setback line and the street right-of-way line.

YARD, REAR: An unoccupied space on the same lot with the principal building, situated between the rear building setback line and the rear property line, and extending the full width of the lot.

YARD, SIDE: An unoccupied space on the same lot with the principal building, situated between the building and the side property line, and extending from the front yard to the rear yard.

ARTICLE II
PROCEDURE FOR PLAT APPROVAL

The procedure for review and approval of a subdivision plat shall consist of two (2) steps. The initial step shall be the preparation and submission to the Planning Commission of a preliminary sketch plat of the proposed subdivision. The second step shall be the preparation and submission to the Planning Commission of a final plat, together with all the required certificates. This final plat shall be recorded in the office of the Chancery Clerk after having been signed by the Secretary of the Planning Commission and accepted by the Mayor and Board of Aldermen.

The subdivider is seriously advised to consult early and informally with the Planning Commission and its technical staff to obtain advice and assistance before he begins to prepare the preliminary plat and make a formal application for approval. This will enable him to become thoroughly familiar with these regulations, the major thoroughfares plan, the zoning ordinance, and other official plans or public improvements which might affect the area he proposes to subdivide. Such informal review may help to prevent unnecessary revisions, the cost of which must be borne by the subdivider.

A. General

1. Any owner of land lying within the Yazoo City corporate limits who wishes to divide such land into two (2) or more lots, sites, or divisions, each lot consisting of less than three (3) acres, for the purpose, whether immediate or future, of sale or building development, or who wishes to resubdivide for this purpose, must submit a plat of such proposed subdivision to the Yazoo City Planning Commission for approval, and must obtain such approval prior to submitting his subdivision plat to the Chancery Clerk for recording. All subdivision plats must conform to the minimum standards of design set forth in Article III of these standards, and must be presented in the manner specified in the following sections of this Article. No plat of a subdivision of land within the Yazoo City corporate limits shall be filed or recorded by the Chancery Clerk without the prior approval of the Yazoo City Planning Commission and Mayor and Board of Aldermen as specified herein.

2. An exception to the procedure specified in the previous paragraph will be permitted only after the following conditions are met by the subdivider:
 - a. The land subdivided is located along a publicly dedicated street and is owned by the subdivider.
 - b. Each new lot contains a minimum of twelve thousand (12,000) square feet and fronts on the publicly dedicated street for a minimum distance of eighty (80) feet.
 - c. The subdivider appears before the Planning Commission and presents his development plan for the land proposed to be subdivided.
 - d. The plans of the subdivider for the subdivision of all lands he owns along publicly dedicated streets conform with the comprehensive plan for the City of Yazoo City.
 - e. The subdivider provides adequate periodic easements (for future streets and utility easements) as specified by the Planning Commission to permit subdivision of the interior land area which might otherwise be landlocked.
3. In order to secure review and approval of a proposed subdivision by the Planning Commission, the prospective subdivider shall, prior to the making of any street improvements or installation of utilities, submit to the Planning Commission a preliminary plat as provided in Section B of this Article. After approval of said preliminary plat, he may proceed with the preparation of the final plat and other documents required in connection therewith specified in Section C of this Article, and the improvements specified in Article IV.

B. Preliminary Plat

- ~~1.~~ At least ten (10) days prior to the meeting at which the preliminary plat is to be considered, the subdivider shall submit to the Planning Commission five (5) copies of a preliminary plat of the proposed subdivision, drawn to a scale of one (1) inch equals one hundred (100) feet.
2. A fee of ten (10) dollars, plus fifty (50) cents per lot contained in the proposed subdivision up to fifty (50) lots, must accompany the request to cover administrative expenses.

PRELIMINARY PLAT MUST CONT.

LOCATION MAP _____

LOCATIONS OF ALL UTILITY EASE

LEGAL DESCRIPTION OF PROPERTY

SCALE OF PLAT, NORTH ARROW, D

NAMES AND RIGHT-OF-WAY WIDTHS

STREETS

SITE DATA - GROSS ACREAGE, NW

AREA IN PARES, AREA IN N

USES

RESERVED SITES (STATE PURPOSE

EXISTING PHYSICAL FEATURES OR

STRUCTURES

NAMES OF ADJOINING PROPERTY O

CONVEYES (AT ACCEPTABLE 10000

BOUNDARY LINES AND DISTANCE

LOCATIONS OF ALL MONUMENTS _____

_____ WITH DISTANCE _____

EXISTING DIVISION OF ALL ADJACENT

NAMES OF DIVISIONS _____

3. The preliminary plat, meeting the minimum standards of design set forth in Article III and the general requirements for the construction of public improvements set forth in Article IV, shall contain the following information, insofar as possible (see Figure 1):
- a. Proposed subdivision's name and location; the names(s) of the owner or owners; the name of the designer (subdivider) of the plat; and the name and registration number of the engineer or surveyor who prepared the plat.
 - b. Date, approximate north point, and graphic scale.
 - c. Location of existing platted property lines, streets, railroad lines or easements, bridges, buildings, water courses, sewers, culverts, drain pipes, water mains; all public utility easements; the present zoning classification, if any, of the land to be subdivided; the zoning classification on all contiguous lands; and the names of adjoining property owners or subdivisions.
 - d. Plans of proposed utility layouts (sewers, water, gas, electricity), showing feasible connections to existing or proposed utility systems. When such connections are not practical, any proposed individual water supply and/or sewage disposal system must meet the approval of the State Health Department.
 - e. Proposed road names; and the locations and dimensions of proposed roads, alleys, easements, parks, and other open spaces or reservations; lot lines; building setback lines; and public utility distribution lines.
 - f. Contours at vertical intervals of not more than five (5) feet, except when specifically not required by the Planning Commission.
 - g. Acreage of the land to be subdivided.
 - h. Location sketch map showing the relationship of the subdivision site to the surrounding area.
 - i. If any portion of the land being subdivided is below the elevation of the regional floodplain (as defined in Article III, Section E), if known, the limits of such flood shall be shown on the plat.

HAS 60 DAYS TO REVIEW

4. Within sixty (60) days after submission of the preliminary plat, the Planning Commission will review it and indicate their approval, disapproval, or approval subject to any required modifications. If a plat is disapproved, the reasons for such disapproval shall be stated in writing. If approved subject to modifications, the nature of the required modifications shall also be stated in writing.
5. One (1) copy of the preliminary plat shall be retained in the Planning Commission files, and one (1) copy shall be returned to the subdivider at the time of approval or disapproval, along with the specific notations of any changes or modifications required.
6. Failure of the Planning Commission to consider any preliminary plat within sixty (60) days after submission of the preliminary plat shall authorize the submission of the plat directly to the Mayor and Board of Aldermen by the applicant.
7. Approval of the preliminary plat by the Planning Commission shall not constitute acceptance of the final plat, except when the final plat is completed during the specified time, in substantial agreement with the layout shown on the preliminary plat.
8. Approval of the preliminary plat shall lapse unless a final plat in substantial agreement therewith is submitted for approval within twelve (12) months from the date of such approval, unless an extension of time is specifically applied for by the subdivider and expressly granted by the Planning Commission.

C. Final Plat

1. The final plat shall conform substantially to the preliminary sketch plat as approved. If desired by the subdivider, the final plat may constitute only that portion of the approved preliminary plat which he proposes to record and develop at that time, provided that such portions must conform to all requirements spelled out by these regulations.
2. At least ten (10) days prior to the meeting at which the final plat is to be considered, the subdivider shall submit the original drawing of the plat done in black drawing ink, along with five (5) copies (black and white or blue line prints), together with any street profiles or other plans which may be required by the Planning Commission.

3. The final plat shall be drawn to a scale of one (1) inch equals one hundred (100) feet, on sheets not larger than eighteen (18) by twenty-four (24) inches. When more than one (1) sheet is required, an index sheet of the same size shall be filed as a key, showing the entire subdivision, with the sheets lettered in alphabetical order.

4. When the plat is approved by the Planning Commission, one (1) copy with the approval of the Planning Commission certified thereon shall be returned to the subdivider to be used for filing with the Chancery Clerk as the official plat of record. The original tracing containing all required certifications shall be returned to the subdivider for his records. Another copy shall be retained in the records of the Planning Commission.
5. The Planning Commission must approve or disapprove a final plat within sixty (60) days after its submission. Failure of the Planning Commission to act on a final plat submission within these sixty (60) days shall authorize the submission of the plat directly to the Mayor and Board of Aldermen by the applicant. If the plat is disapproved, the grounds for such disapproval shall be stated in writing and submitted to the Mayor and Board of Aldermen for inclusion in the minutes of their meeting.
6. Before the final plat is approved, the landowner must provide the Planning Commission with an abstract of title showing that the applicant is the legal owner of the land.
7. Approval of a final plat by the Planning Commission shall not constitute acceptance of the final plat by the city, or acceptance by the city of the dedication of any street or other public way or ground. After approval of the final plat and the construction of streets shown thereon, the Planning Commission may recommend to the Yazoo City Mayor and Board of Aldermen that they accept these streets as public roads, and take over their perpetual maintenance.
8. The final plat shall contain the following information (Figure 2):
 - a. Lines of all streets, roads, and alleys; building setback lines; lot lines; all lots numbered sequentially; all reservations for public easements; all areas to be dedicated for public use or for other than residential purposes, with notes stating their intended purpose or any proposed limitations.
 - b. Sufficient data to determine readily and reproduce on the ground the location, bearing, and length of every road line, lot line, boundary line, block line, and building line, whether curved or straight. This necessary information shall include the radius, central angle, and tangent distance for the center line of all curved roads and property lines which are not the boundary lines of the property being subdivided.

FINAL PLAT MUST CONTAIN:

NAMES OF ADJACENT SUBDIVISIONS

NAMES AND RIGHT-OF-WAY WIDTHS OF ALL
STREETS

LOCATIONS, DIMENSIONS, AND PURPOSE OF
ALL EASEMENTS

LOCATIONS, DESCRIPTIONS, AND CERTIFICATION
OF MONUMENTS, LOT CORNERS, AND OTHER
SURVEY POINTS

SCALE OF PLAT, NORTH ARROW, DATE

TRACT BOUNDARIES AND DIMENSIONS

PURPOSE OF ANY RESERVED SITES

PURPOSE OF ALL DEDICATIONS

SET-BACK LINES

LOT PROPERTY LINES AND DIMENSIONS

LOT IDENTIFICATION NUMBERS

PLAT APPROVAL CERTIFICATIONS

NAME OF SUBDIVISION

RAPID, ARCS, TANGENTS, AND CENTRAL

ANGLES OR ALL OTHER DATA

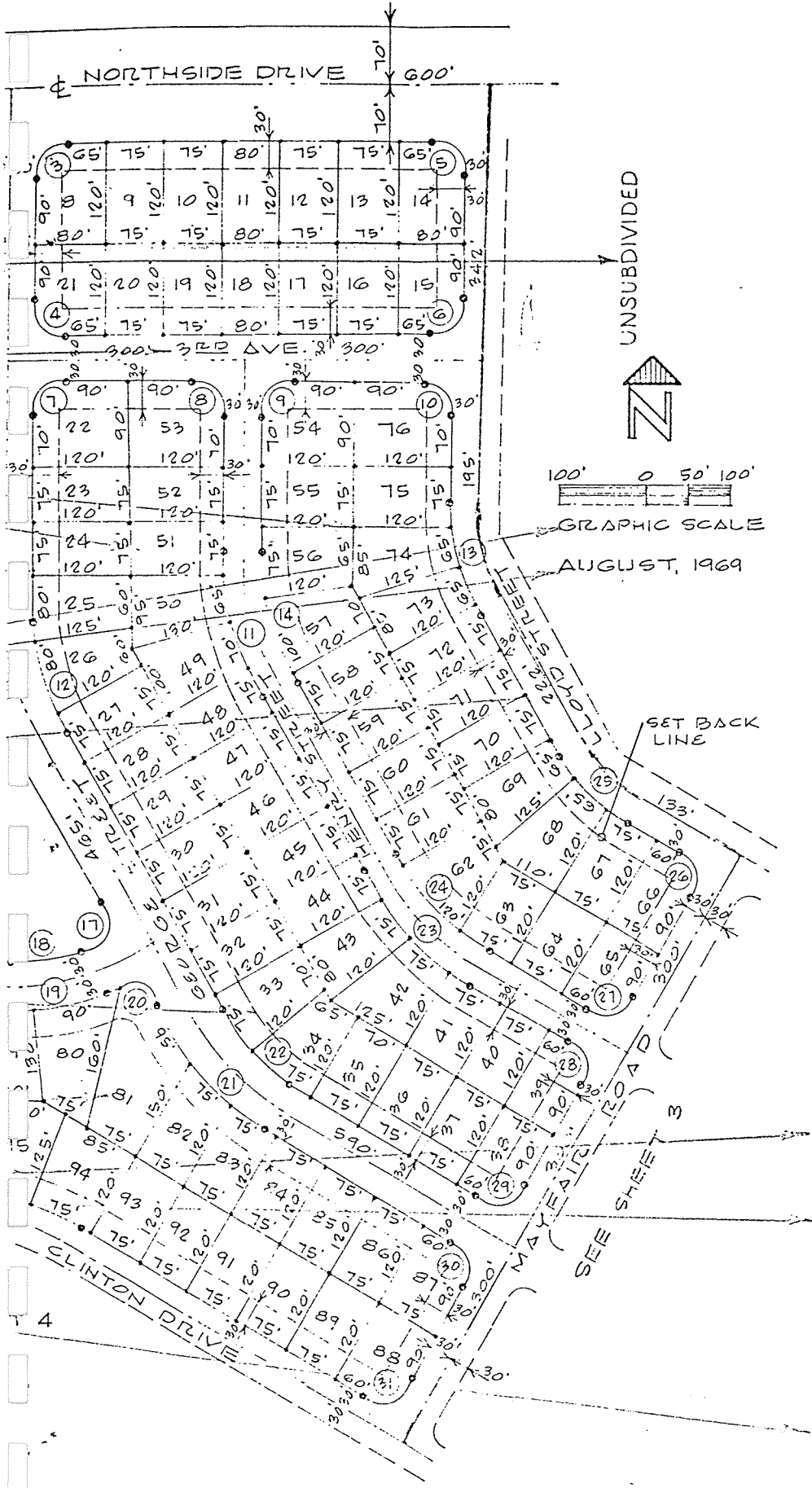
NOTES:

(31) - CURVE DATA REFER TO TABLE ON SHEET 2

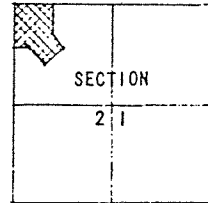
BEARINGS AND DEFLECTION ANGLES FOR EACH LOT ON SHEET 2

- REPRESENTS CORNER MONUMENT SET
- REPRESENTS 5/8" IRON PIN SET





NORTHSIDE DRIVE



LOCATION MAP

MONUMENT CERTIFICATION

PROFESSIONAL ENGINEER, REGISTRATION NUMBER

CERTIFICATE OF TITLE AND LEGAL DESCRIPTION

TITLE INSURANCE COMPANY

DEDICATION OF STREETS

NAME/S OF OWNER/S

APPROVALS

SECRETARY OF PLANNING BOARD

CITY ENGINEER

CITY HEALTH OFFICER

MAYOR

CITY CLERK

MAYFAIR MANOR

A PART OF THE NW 1/4 OF SEC. 21

T-16-N, R-1-W

HINDS COUNTY, MISSISSIPPI

-ER TO TABLE ON SHEET 2

ON ANGLES FOR EACH LOT ON SHEET 2

MONUMENT SET

IRON PIN SET

- c. Length of all dimensions to the nearest one tenth (1/10) of one (1) foot, and bearings of all angles to the nearest one (1) minute. Total error of linear measurement must not exceed one (1) foot per five thousand (5,000) feet.
 - d. Location and description of all iron pins.
 - e. Name and location of adjoining subdivision roads, and the location and ownership of adjoining unsubdivided property.
 - f. Date, title, name and location of subdivision, graphic scale, and true north point.
 - g. Location sketch map showing the relationship of the subdivision site to the surrounding area.
 - h. If any portion of the land being subdivided is below the elevation of the regional floodplain (as defined in Article III, Section E), if known, the limits of such flood shall be shown on the plat.
9. The following certificates shall be presented together with the final plat:
- a. Certificate showing that the applicant is the legal owner of the land, and that he formally dedicates all streets, rights-of-way, and any other sites for public use (Form 1 in Appendix A).
 - b. Certificate by the registered surveyor or engineer of the accuracy of the survey and plat, and the placement of all the required monuments (Form 2 in Appendix A).
 - c. Certificate by the Yazoo City Street Superintendent that the subdivider has complied with one of the following alternatives (Form 3 in Appendix A):
 - (1) installed all improvements according to the requirements in these regulations; or
 - (2) posted a security bond or certified check, or established an escrow account in an amount sufficient to assure the completion of all required improvements.
 - d. Certificate showing approval by the Yazoo County Electric Power Association, the Public Service Commission or Mississippi Power Light Company of the location of all electric power easements within the subdivision (Form 4 in Appendix A).

- e. Certificate by the appropriate authority of the approval of the water and gas distribution systems within the subdivision (Form 5 in Appendix A).
- f. Certificate by the Sewerage Superintendent of the approval of the sewage collection system and other appurtenances within the subdivision (Form 6 in Appendix A).
- g. Certificate showing approval of the plat by the Planning Commission, to be signed by the Secretary of the Yazoo City Planning Commission (Form 7 in Appendix A).
- h. Certificate of Comparison, to be signed by the engineer or surveyor and the Chancery Clerk, certifying that the final plat presented for recording is a true and correct copy of the plat as approved (Form 8 in Appendix A).
- i. Certificate of Compliance with the Subdivision Regulations, to be signed by the engineer or surveyor (Form 9 in Appendix A).
- j. Certificate of Acknowledgement of the signature of the engineer or surveyor of the plat, executed by a notary public (Form 10 in Appendix A).

ARTICLE III

GENERAL REQUIREMENTS AND MINIMUM STANDARDS OF DESIGN

A. Roads

1. Conformity to the Major Thoroughfares Plan

The location and width of all streets and roads shall conform to the official major thoroughfares plan.

2. Relation to Adjoining Road System

The proposed street system shall extend existing or proposed roads where desirable, and these shall be extended at a width no less than the required minimum width.

3. Road Elevations

The Planning Commission may require profiles and elevations of roads in areas characterized by steep terrain or subject to flooding. Fill may be used to improve street grades, provided such fill does not unduly increase flood heights. Also, all drainage openings shall be designed so as not to restrict the flow of water or unduly increase flood heights.

4. Road Widths

The minimum width of road rights-of-way, measured from lot line to lot line, shall be shown on the official major thoroughfares plan, or if not shown on such plan, shall be not less than the following:

a. Primary and Secondary Streets and Highways - 100 feet

Primary and secondary streets and highways are those designed to be used primarily for fast or heavy traffic, and shall be shown on the major thoroughfares plan. (See Figure 3)

b.

Collector Streets - 60 feet

Collector streets are those which carry traffic from minor streets to the major system of arterial streets and highways, and include the principal entrance roads of residential developments, as well as streets used for major circulation within the development. (See Figure 3)

c. Minor Residential Streets - 50 feet

Minor residential streets are intended primarily to provide access to abutting residential properties, and are designed so as to discourage their use by through traffic. (See Figure 3)

d. Marginal Access Streets - 60 feet

Marginal access streets are minor streets located adjacent to and parallel with arterial roads and highways, and are designed to provide access to abutting properties and discourage through traffic. (See Figure 3)

e. Dead-End Streets (cul-de-sacs) - 60 feet

Cul-de-sacs are permanent dead-end streets or courts which provide access to a small number of residential properties, and are designed so that they cannot be extended in the future. (See Figure 3)

f. Alleys - 20 feet

Alleys are minor public ways intended primarily to provide service access to the back or side of properties otherwise abutting on a public road. (See Figure 3)

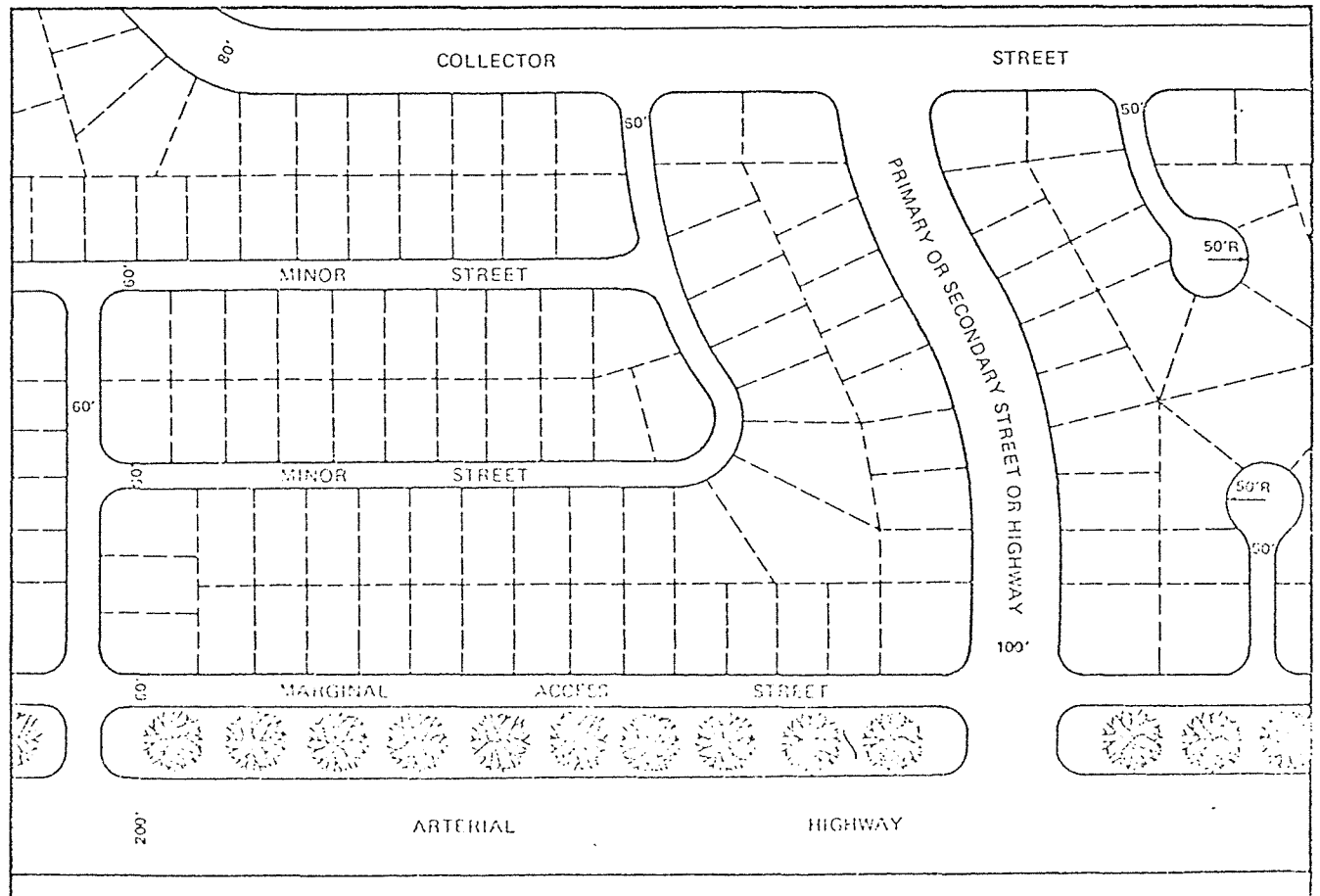
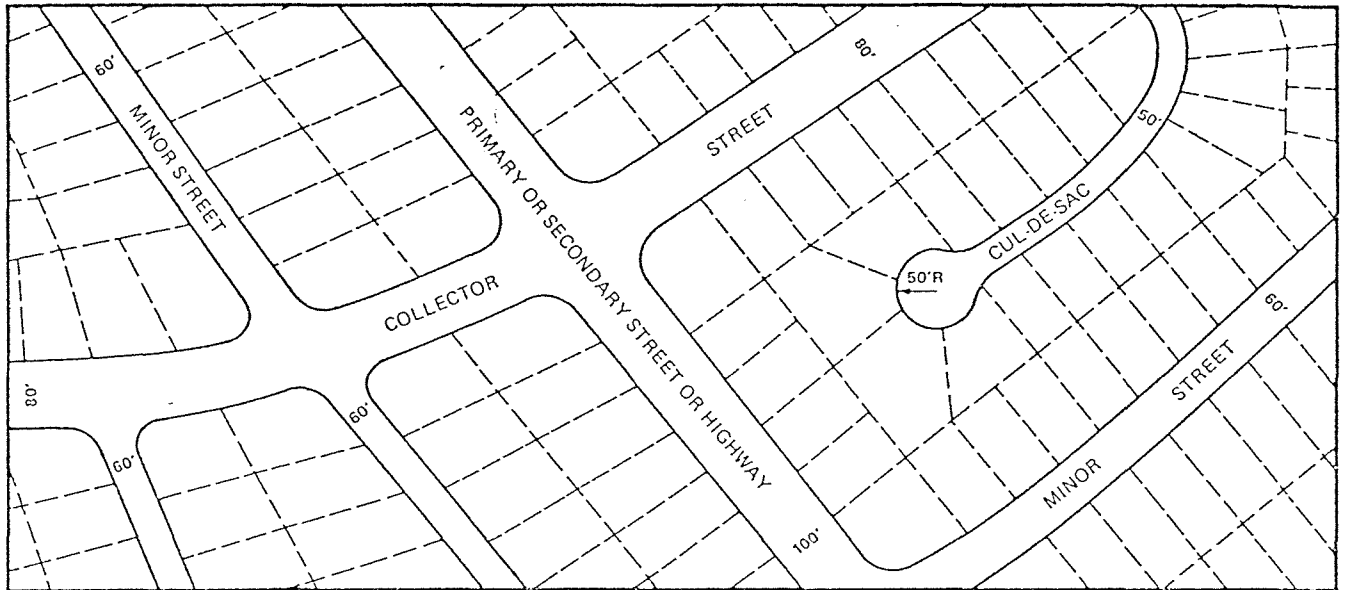
In cases where topography or other physical conditions make a street of the required minimum width impracticable, the Planning Commission may modify the above requirements. Through proposed neighborhood and local business areas, the road widths specified above shall be increased ten (10) feet on each side to facilitate the convenient movement of vehicles into and out of necessary off-street parking areas without causing any interference in the flow of traffic.

5. Additional Width on Existing Streets

Subdivisions that adjoin existing streets shall dedicate additional rights-of-way to meet the above minimum road width requirements.

- a. The entire right-of-way shall be provided where any part of the subdivision is located on both sides of the existing street.

Figure 3
STREET TYPES AND WIDTHS



- b. When the subdivision is located on only one (1) side of an existing street, one-half ($\frac{1}{2}$) the required right-of-way, measured from the center line of the existing roadway, shall be provided.

6. Restriction of Access

When a tract fronts on an arterial street or highway, the Planning Commission may require all lots subdivided to be provided with frontage on a marginal access street.

7. Road Grades

Maximum grades on major streets shall be six (6) percent. Maximum grades on other streets may exceed six (6) percent, but not ten (10) percent, except where the topography is excessively steep and the necessary road cuts cannot be made. Minimum grades on all roads must be at least one-half ($1/2$) of one (1) percent to ensure adequate drainage.

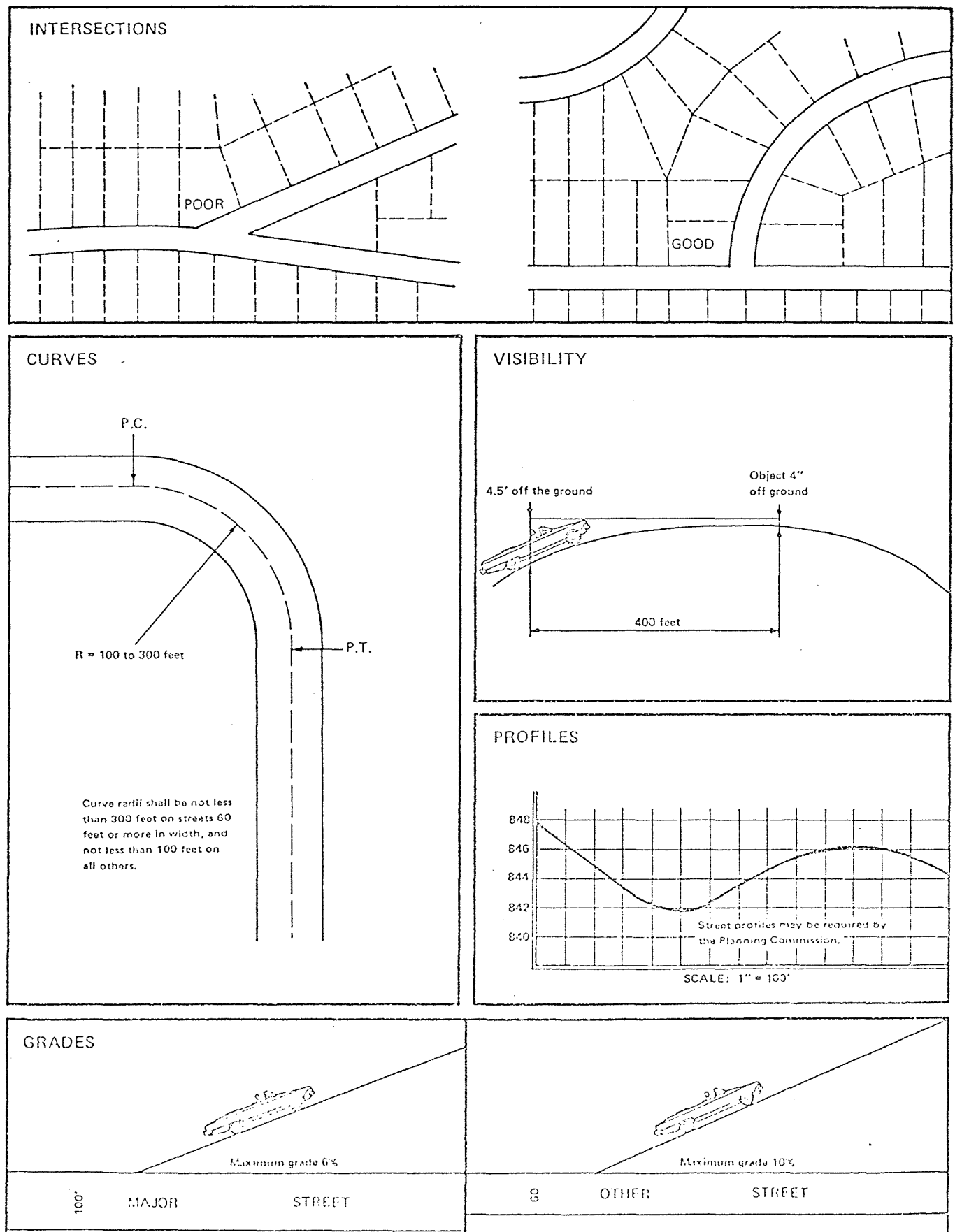
8. Intersections

Road intersections shall be as nearly at right angles as possible, and no intersection shall be permitted at an angle of less than sixty (60) degrees. Property line radii at street intersections shall not be less than twenty (20) feet, and where the angle of street intersection is less than seventy-five (75) degrees, the Planning Commission may require a greater curb radius. The property line at such street corner shall be rounded or otherwise set back sufficiently whenever necessary to permit the construction of a curb having a desirable radius without curtailing the sidewalk at a street corner to less than the normal width.

9. Vertical Curves

Every change in grade shall be connected by a vertical curve constructed so as to afford a minimum sight distance of four hundred (400) feet, measured from the driver's eyes, which are assumed to be four and one-half ($4\frac{1}{2}$) feet above the pavement surface, to an object four (4) inches high on the pavement located four hundred (400) feet ahead. Profiles of all streets showing natural and finished grades drawn to a scale of not less than one (1) inch equals one hundred (100) feet horizontally and one (1) inch equals twenty (20) feet vertically may be required by the Planning Commission. (See Figure 4.

Figure 4
INTERSECTION, CURVE, GRADE, PROFILE, AND VISIBILITY REQUIREMENTS



10. Tangents

A tangent at least one hundred (100) feet in length shall be introduced between all reverse curves on all primary, secondary, and collector streets. (See Figure 4.)

11. Road Jogs or Offsets

Road jogs with center-line offsets of less than one hundred twenty-five (125) feet shall not be permitted.

12. Dead-End Streets

- a. Minor terminal roads or courts designed to have one end permanently closed shall be no more than six hundred (600) feet long, unless a greater length is necessitated by the topography. These roads shall be provided at the closed end with a turnaround having an inside roadway diameter of at least eighty (80) feet and a street right-of-way diameter of at least one hundred (100) feet. The Planning Commission may approve an alternate design such as "T" or "Y" intersection at the end of the roadway.
- b. Where, in the opinion of the Planning Commission, it is desirable to provide street access to adjoining unsubdivided property, proposed street rights-of-way shall be dedicated and extended to the boundary of such property. Such dead-end streets shall be provided with a temporary turnaround having an inside roadway diameter of at least eighty (80) feet.

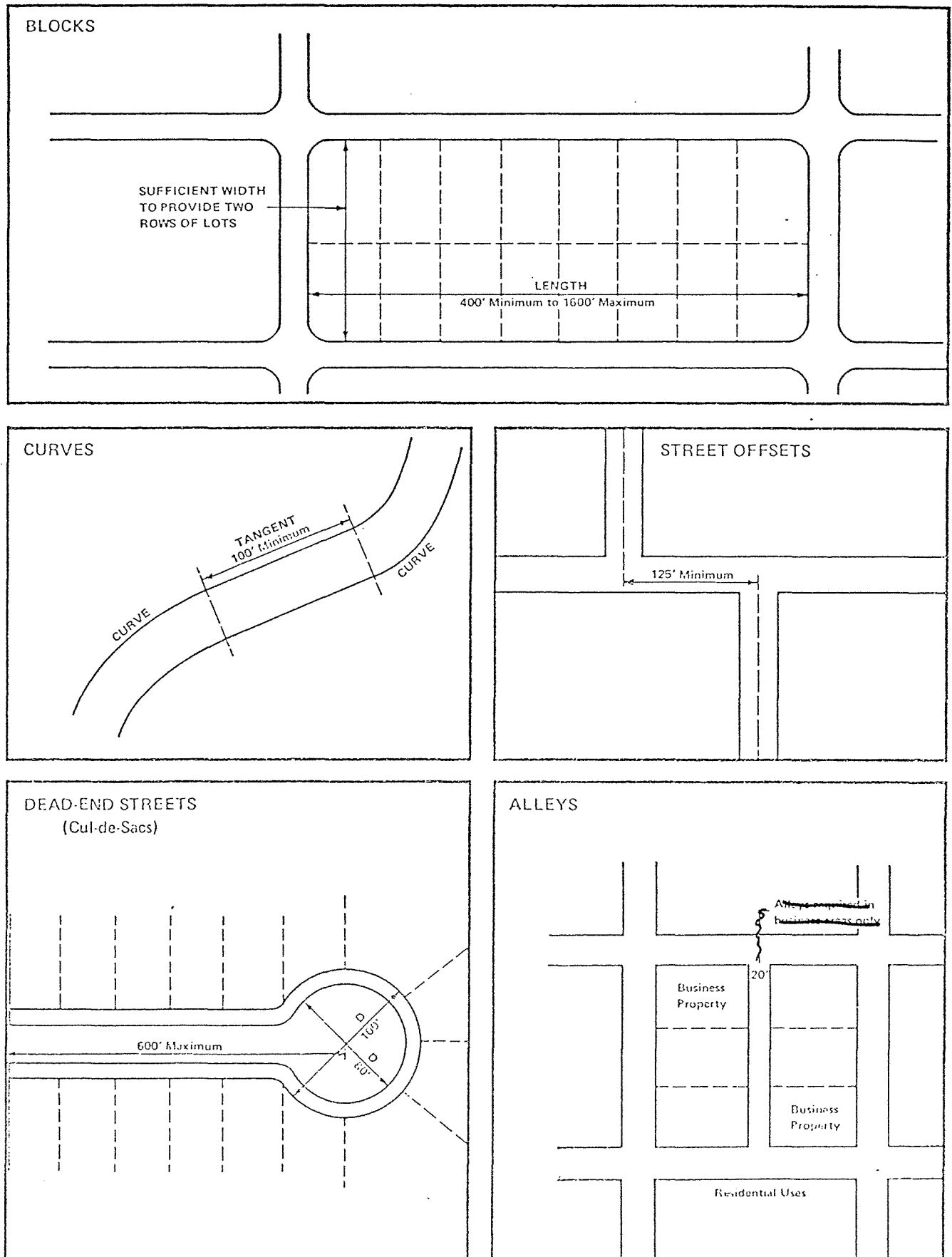
13. Private Streets and Reserve Strips

There shall be no private street platted in any subdivision. Every lot in every subdivision shall front upon a publicly dedicated street. There shall be no reserve strip controlling access to any street, except where the control of such strip belongs to the community under conditions approved by the Planning Commission. (See Figure 5.)

14. Street Names

Proposed roads which are obviously in alignment with others already existing and named shall bear the names of existing streets. In no case shall names of proposed streets duplicate existing street names, regardless of the addition of the suffix street, avenue, boulevard, drive, way, place, lane, or court. Through its index list of street names on file, the

Figure 5
BLOCK, CURVE, DEAD-END, OFFSET, AND ALLEY REQUIREMENTS



Planning Commission shall help the subdivider avoid duplication of street names.

15. Alleys

Alleys shall be provided to the rear of all lots used for business purposes, and ~~should~~^{need} not be provided in residential areas where the subdividers produce evidence satisfactory to the Planning Commission of the lack of need for alleys.

B. Blocks

1. Length

Blocks shall be not less than four hundred (400) nor more than sixteen hundred (1,600) feet in length (figure 5), except where the Planning Commission considers it necessary to secure a more efficient use of the land or a desired street pattern. Where a block is longer than one thousand (1,000) feet, the subdivider may be required to provide a ten (10) foot improved crosswalk at mid-block to facilitate pedestrian movement.

2. Width

Blocks shall be wide enough to allow two (2) rows of lots except where reverse frontage on major streets is provided, or where prevented by topographic conditions. In such case the Planning Commission may approve a single row of lots of adequate depth. (See figure 5.)

C. Lots

1. Adequate Building Sites

Each lot shall contain a building site that is free from flooding and that does not lie within the limits of any existing easement or building setback line required by Section C, Subsection 4 of this article.

2. Arrangement

Insofar as possible, side lot lines shall be at right angles to straight street lines and radial to curved street lines. Each lot must front for a minimum distance of eighty (80) feet on a public dedicated street, with the eighty (80) foot minimum measured at the front building setback line.

3. Minimum Size

The size, shape, and orientation of lots shall be as the Planning Commission deems appropriate for the type of development and use contemplated.

- a. Residential lots served by the public water and sanitary sewer systems shall be not less than seventy (70) feet wide at the front building setback line, and not less than eight thousand (8,000) square feet in area.
- b. The size of lots reserved or platted for commercial or industrial use shall be adequate to provide the off-street service and parking facilities required by the type of use and development contemplated. Platting of individual lots shall be avoided in favor of an overall design of the land to be used for such purposes.

4. Building Setback Lines

The minimum building setback shall be in strict compliance with the Zoning Ordinance section for the applicable zone classification.

D. Recreational Areas

The subdivider shall be required to reserve ten percent (10%) of the area of any subdivision for public use as a recreational area. The ten percent to be reserved will be designated by the Planning and Zoning Commission upon review of the preliminary plat, provided however, that a new area may be designated upon subsequent submission of a revised plat. The City of Yazoo City shall have the option to purchase the reserved ten percent of the subdivision for use as a recreational area. The option to purchase shall run for a period of three years following completion of the improvements in the subdivision and acceptance of these improvements by the City. Should the City fail to purchase the reserved property within the three year option period, or reject the purchase option prior to expiration of three years, the subdivider shall be free to dispose of the reserved property. Should the City exercise the option to purchase the reserved property, the purchase price shall be the current market value established at the time of submission of the final plat.

E. Utility Easements

Except where alleys are specifically provided, the Planning Commission shall require utility easements of five (5) feet in width for poles, wires, conduits, storm and sanitary sewers, gas, water and heat mains, or other utility lines along the rear of all lot lines. Easements of the same or greater width may be required in some special cases along side lot lines or across lots for the extension of existing or planned utilities.

F. Suitability of the Land

The Planning Commission shall not approve the subdivision of land if, after adequate investigations have been conducted by the public agencies concerned, it is determined that in the best interest of the public the particular site is not suitable for the type of platting or development proposed. Land subject to flooding, or land deemed to be topographically unsuitable shall not be platted for residential occupancy nor for any other use which may increase the flood hazard, aggravate erosion, or endanger health, life, or property. Such land within any plat shall be set aside for uses that do not produce unsatisfactory living conditions.

Fill may not be used to raise the level of land in areas subject to flood, unless the fill proposed does not restrict the flow of water or unduly increase flood heights.

G. Variances

The subdivider or his authorized representative may apply to the Board of Adjustment by submitting to its chairman a written request for a variance of some of the provisions specified in these regulations. The Board of Adjustment may recommend granting a variance, subject to final approval by the Mayor and Board of Aldermen, only under the following conditions:

1. Where the subdivider can show that strict adherence to these regulations would cause unnecessary hardship.
2. Where the Board of Adjustment decides that there are topographic or other conditions peculiar to that site, and where a minor departure from these regulations will not destroy the intent of these regulations. Any variance thus granted shall be stated in writing in the minutes of the Board of Adjustment, with the reasons for granting the variance also stated in writing.

H. Zoning or Other Regulations

No final plat of land within the force and effect of these regulations shall be approved unless it also conforms to all zoning regulations applicable in the area in which the subdivision is planned.

In all cases where there is a discrepancy between minimum standards or dimensions noted herein and those contained in zoning ordinances, building codes, or other official regulations, the highest standard shall apply in all cases.

ARTICLE IV

DEVELOPMENT PREREQUISITE TO FINAL APPROVAL

A perfectly prepared and recorded subdivision plat means little to a prospective lot buyer until he can see actual physical transformation of raw acreage into lots suitable for building purposes and human habitation. Improvements by the subdivider spare the community a potential tax liability. The following tangible improvements or provision for their installation are required before final plat approval, in order to assure the physical reality of a subdivision which approval and recordation of the final prepared plat will establish legally.

A. Required Improvements

Every subdivision developer shall be required to grade and improve streets and alleys (except those streets or alleys that have been accepted by the Street Superintendent prior to the adoption of these subdivision regulations) and install monuments, sanitary sewers, storm drainage facilities, water mains, and sidewalks, according to the following specifications.

1. Monuments

- a. Numbered concrete monuments four (4) inches in diameter or square and three (3) feet long with a flat top shall be set at all street corners, at all points where the street lines intersect the exterior boundaries of the subdivision, at the intersection of curves and tangents along street lines, and at all corners of the plat. The top of each monument shall have an indented cross to identify properly the location and shall be set flush with the finished grade.
- b. All other lot corners shall be marked with iron pipe not less than three-fourths (3/4) inches in diameter and twenty-four (24) inches long, driven so as to be flush with the finished grade.

2. Storm Drainage

An adequate drainage system, including necessary open ditches, pipes, culverts, intersectional drains, drop inlets, bridges, and so forth, shall be provided by the subdivider for the proper drainage of all surface water. Cross drains shall be provided to accommodate all natural water flow, and shall be of sufficient length to permit the construction of a full width

roadway, including all the required slopes. The size openings to be provided shall be determined by Talbot's formula, using a runoff factor (C) of not less than five tenths (0.5), or other accepted engineering practice, but in no case shall the pipe be less than twelve (12) inches in diameter. The design of the total drainage system shall be based on not less than the runoff from a storm with a ten (10) year return frequency. Where any natural watercourse is to be disturbed or blocked, adequate provision shall be made to handle the water affected. The subdivider shall provide Portland cement concrete curbs (See Appendix C), the face of which shall not be less than six (6) inches in height with backfill sloping toward the curb but higher than the curb to insure drainage of surface water into the storm drainage system.

3. Street Development

The following standards will be required for the preparation of the subgrade and the primary and surface treatment to be applied to all subdivision streets.

a. Subgrade

All streets, roads, and alleys shall be graded to their full width by the subdivider so that pavements and sidewalks (where required) can be constructed on the same level plane. In preparing the subgrade, all boulders, tree stumps, organic material, soft clay, spongy material, and other objectionable material shall be removed to a depth of at least two (2) feet below the graded surface in cuts, and two (2) feet below the natural ground in fills. All objectionable matter shall be removed from within the road right-of-way limits and disposed of in such a manner that it will not become incorporated in the material used for road fills, nor in any manner hinder proper operation of roadway ditches or the drainage system. Rock, when encountered, shall be scarified to a depth of twelve (12) inches below the subgrade. The subgrade shall be properly shaped, rolled, and uniformly compacted to conform with the accepted cross section and grades.

Fill shall be spread in layers not to exceed twelve (12) inches loose, and compacted in an appropriate manner. Utility trenches or other

places not accessible to a roller shall be mechanically tamped. Whenever water is used to assist compaction, the water content shall not exceed the optimum of moisture.

b. Base

The type of base to be placed on roads or streets shall be of no lower classification than "water-bound macadam" or its equivalent. Base course material shall consist of durable particles or fragments of stone, free from an excess of flat, elongated, soft, or disintegrated pieces, dirt, or other objectionable matter. The stone shall be that generally known as "crusher run stone," or Michie or Tishomingo Gravel. All base course material shall be deposited and spread by means of spreader boxes or approved mechanical equipment, or from moving vehicles equipped to distribute the material in a uniform layer. Base course material shall be dampened and rolled by an approved roller of a size adequate to obtain proper compaction. The completed compacted base shall be no less than eight (8) inches thick and no less than thirty-four (34) feet wide.

No base course material shall be deposited or shaped when the subgrade is frozen or thawing, or during unfavorable weather conditions.

c. Prime Coat

After a thoroughly compacted base has been established, a prime coat shall be applied as follows:

With a temperature of thirty-five (35) degrees and rising, tar grade RT-2 to RT-1 inclusive, or MC-1 or MC-2 shall be applied at the rate of four-tenths ($4/10$) gallon per square yard of base surface. Then, stone chips graded from one-half ($1/2$) inch down to #8, with no dust, shall be applied at the rate of eight (8) to ten (10) pounds per square yard, rolled until thoroughly compacted, and left to cure for such time as the Street Superintendent may direct, but in no case shall the period required for curing be less than seven (7) days.

d. Wearing Surface

The wearing surface shall consist of or be equivalent to one of the following types of surface treatments:

One Course Asphaltic Concrete (Plant Mixed Surface Treatment) - Details of the composition and construction method for this wearing surface are presented in Appendix D.

Other types of wearing surfaces, including concrete and soil cement, may be used to meet the requirements of these subdivision regulations, subject to the approval and conditions established by the Planning Commission.

4. Required Pavement Widths

Required widths for the surface treatment of roadways will necessarily vary with the character of building development and the amount of traffic anticipated. Minimum widths for surface treatment of streets are indicated below. Some of these are shown in the cross-sections on Figure 6.

	Minimum Pavement Width (Feet)
Primary and secondary streets and highways	variable
✓ Collector streets	48
Minor residential streets	32
Marginal access streets	24
Dead-end streets (cul-de-sacs)	24
Alleys	20

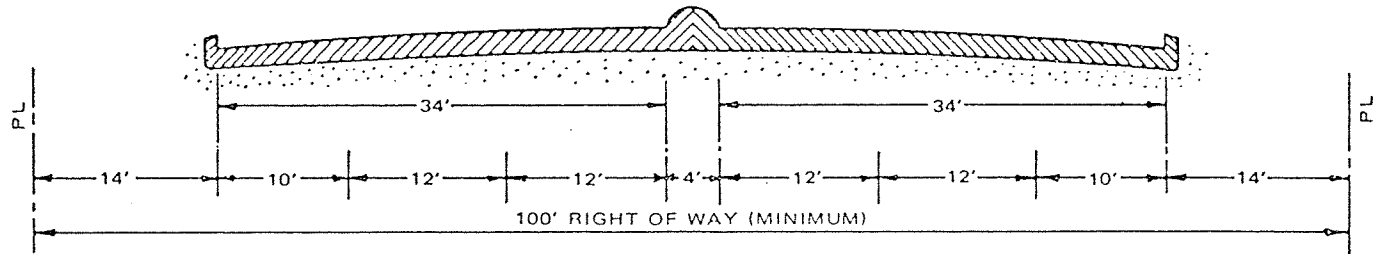
5. Sanitary Sewers

When located within the service area of a public sewer system, sanitary sewers shall be installed in such manner as to adequately serve all lots, and shall be connected to the public system.

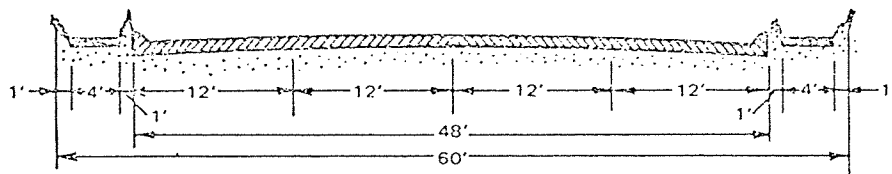
A means of collecting and transporting sewage from each lot to a sewer main in the interceptor sewer system maintained by the municipality shall be provided through a system of appropriate sewer mains and appurtenances beginning at the nearest municipal sewer and terminating at the front lot (street) property line, unless topography or necessity makes service at some other location on the lot necessary.

The sewer system installed shall be capable of transporting the accumulated volume of sewage (calculated at a rate of eight hundred (800) gallons per

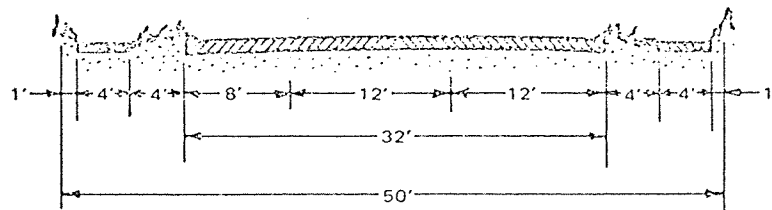
Figure 6
RECOMMENDED STREET CROSS SECTIONS



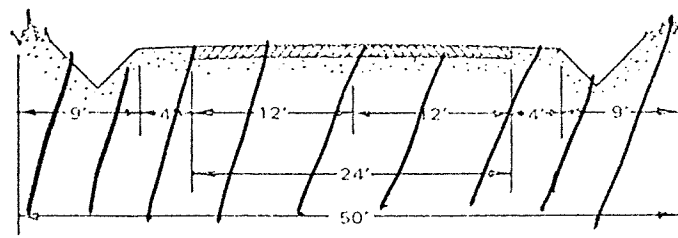
PRIMARY AND SECONDARY STREETS



COLLECTOR STREETS



MINOR RESIDENTIAL STREETS
(CURB AND GUTTER)



Minimum Right-of-Way (50' and 60')

lot per day) to be contributed by the occupants of each lot in the subdivision at a minimum velocity of two (2) feet per second. All sewer mains shall be connected to the municipality's sewage collection and transport system, and shall be designed to provide any additional capacity needed to adequately serve properties located uphill in the particular drainage basin served by the sewer main.

All elements of any sewage collection and transport system connected to the municipality's sewer system shall be installed according to the specifications of the Mississippi Air and Water Pollution Control Commission, The Mississippi State Board of Health, or both, if necessary, and shall be approved by them. Design criteria and parameters used in the design and construction of the system shall be the same as those outlined in the latest published edition of the American Society of Civil Engineers' Manual No. 37, Design and Construction of Storm and Sanitary Sewers (Water Pollution Control Federation Manual No. 9). All materials used shall comply with the appropriate standards and requirements specified in the latest published edition of the American Society of Testing Materials.

All collection laterals, mains, and system appurtenances shall be placed in the utility easement or other approved location as may be required to provide customer service and permit the maintenance of mains. Such collection appurtenances may be located under a paved street if topography dictates, and may also have to be located outside of street rights-of-way if the topography of the area so requires.

All sewer service laterals shall terminate at the front lot line, unless topography dictates service from some other location. Service laterals shall be installed prior to the construction of the base course of the street in which the service laterals will be located.

6. Sidewalks

For the safety of pedestrians in multi-family areas, group housing developments, or commercial areas, the developer will be required to build cement concrete sidewalks to the following specifications:

a. Multi-Family or Group Housing Developments

Install sidewalks at least five (5) feet wide and four (4) inches thick.

b. Commercial Areas

Install sidewalks at least eight (8) feet wide and four (4) inches thick.

Sidewalks shall not be located closer than one (1) foot to the individual property lines, so as to prevent interference or encroachment by fencing, walls, hedges, or other planting or structures placed on the property line at a later date. Portland cement concrete sidewalks shall be installed according to the specifications presented in Appendix E.

7. Installation of Underground Utilities

After grading is completed and approved and before any base is applied, all underground work (water mains, gas mains, sewer mains, and so forth) and all service connections shall be installed completely and approved throughout the length of the road and across the flat section. Where the utility mains are off the pavement, the developer may elect to omit the installation of service connections, provided that when these service connections are needed they are extended across the street without breaking or weakening the existing pavement. Where rock is known to exist beneath the pavement at a depth which would interfere with the jacking of service connections, the Planning Commission shall require the complete installation of service connections before any road base is applied. In business sections where there are no grass plots, utility mains shall be installed under the sidewalks wherever possible.

8. Water Supply System

Water mains properly connected with the public water supply system or with an alternate supply approved by the city health officer shall be constructed to adequately serve all lots shown on the subdivision plat for both domestic use and fire protection purposes. Service lines must be installed to the individual property lines. The size and placement of all lines shall conform with accepted standards of good practice for municipal water systems.

A means of delivering potable water from the municipal water system to each lot shall be provided through a system of appropriate pipes and valves, beginning at the nearest municipal water main and terminating at the front lot line where there shall be installed a shut-off valve.

Water mains and appurtenances installed in residential areas shall be capable of delivering at least five hundred (500) gallons of water per minute at times of maximum demand on the municipal water system, at a residual pressure of twenty (20) pounds per square inch, to a fire hydrant located within two hundred fifty (250) feet of any building site in the subdivision.

Water mains and appurtenances installed in areas other than those designated for residential use shall be capable of delivering water at rates and pressures commensurate with the required use and/or fire protection needs established by the municipality.

All water mains shall be connected to the municipality's distribution system and shall be designed to provide additional capacity needed to adequately serve properties located beyond the land area being developed.

All elements of any water distribution system connected to the municipality's shall be installed according to the specifications of the Mississippi State Board of Health, and must be approved by that board. The physical connection between the system installed by the subdivider and the municipal system shall not be made until the system being installed is sterilized and a clearance report obtained from the Mississippi State Board of Health.

Design criteria and parameters used in the design and installation of the system shall be the same as those outlined in the latest published edition of the Recommended Standards for Water Works, distributed by the Great Lakes-Upper Mississippi River Board of State Sanitary Engineers. All materials and supplies shall meet the requirements and standards established in the latest published editions of the American Water Works Association, American Society for Testing Materials, American Standards Association, and, in case of polyvinyl chloride plastic pipe, the standards established by the National Sanitation Foundation and the Society of Plastic Industry.

All distribution mains and system appurtenances shall be installed at a depth of thirty (30) inches or more, and shall be placed in the utility easement or other approved location but not under a paved street, as may be required to provide customer service and permit the maintenance of

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mains. All service lines shall be constructed prior to the construction of the street base course, or shall be installed by use of a boring process which will not disturb the surface of paved streets.

B. Recommended Improvements

1. Street Trees

Although not required by these standards, the planting of street trees is considered a good business practice, and the responsibility of the subdivider.

Street trees protect against excessive heat and glare, and enhance the attractiveness and value of abutting property. The Planning Commission will assist the subdivider in the choice of tree species and in the location of trees.

Trees should be planted five (5) feet inside property lines where they will be less subject to injury and damage by motor vehicles and where they will have more favorable conditions for growth. If trees are to be planted within a planting strip in the right-of-way, the proposed location and species to be used must be submitted to the Planning Commission for its approval, since the public will inherit the care and maintenance of these trees.

All street trees and ornamental shrubs shall be planted outside of utility easements unless the property owner assumes all responsibility for damage to trees or shrubs located in such easements.

C. Guarantee in Lieu of Completed Improvements

No final subdivision plat shall be approved by the Planning Commission or accepted for recording by the Chancery Clerk until all the required improvements have been constructed in a satisfactory manner and approved by the appropriate agent. In lieu of such prior construction, the Planning Commission may accept a security bond, certified check, or establish an escrow account in an amount equal to the total estimated cost of installation of the required improvements, insuring that the improvements will be made and all the utilities installed without cost to the community in the event of default by the subdivider. A sample copy of a performance bond is in Appendix B of these regulations.

ARTICLE V
ENFORCEMENT AND PENALTIES FOR VIOLATION

Enforcement of these regulations and penalties for the unapproved recordation or transfer of land are provided by State law in the authority granted by public acts of the State of Mississippi, as follows:

A. Enforcement

1. No plat or plan of a subdivision of land into two (2) or more lots located within the area of planning jurisdiction shall be admitted to the land records of the county or received or recorded by the Chancery Clerk until said plat or plan has received final approval in writing by the Planning Commission and Mayor and Board of Aldermen as provided by law.
2. No board, public officer, or authority shall light any road, lay or authorize the laying of water mains or sewers, or permit the construction of other facilities or improvements in any road located within the area of planning jurisdiction unless such road shall have been accepted, opened, or otherwise received the legal status of a public road prior to the adoption of these regulations, or unless such road corresponds in its location and alignment to a road shown on a preliminary subdivision plat approved by the Planning Commission, or on the official major thoroughfares plan adopted by the Commission as provided in Title 13, Chapter 1, Section 2890.5, Mississippi Code of 1942, as amended.

B. Penalties

1. No Chancery Clerk shall receive, file, or record a plat of a subdivision within the Yazoo City corporate limits without the prior approval of the Planning Commission and Mayor and Board of Aldermen as required in Title 16, Article VII, Section 3374-123, Mississippi Code of 1942, as amended. Any Chancery Clerk so doing shall be deemed guilty of a misdemeanor, and punished as provided by law.
2. Whosoever being the owner or agent of the owner of any land who transfers, sells, agrees to sell, or negotiates to sell such land by reference to or exhibition of or by other use of a plat or subdivision of such land without first having submitted a plat of such subdivision to the Planning Commission and obtained its approval as required before such plat can be recorded in the office of the appropriate Chancery Clerk, shall be deemed guilty of a

misdemeanor, and punished as provided by law. The description by metes and bounds in the instrument of transfer or other document used in the process of selling or transferring such land shall not exempt the transaction from such penalties. The City Attorney or other official designated by the Mayor and Board of Aldermen shall have the authority to prevent such transfer, sale, or agreement by injunction or other appropriate legal action.

3. Any building or structure erected or to be erected in violation of these subdivision regulations shall be deemed an unlawful building or structure, and the Building Inspector, City Attorney, or other official designated by the Mayor and Board of Aldermen may bring action to enjoin such erection, or cause it to be vacated or removed.
4. Any person, corporation, partnership, or association of persons violating any of the provisions of these regulations shall be guilty of a misdemeanor, and upon conviction shall be fined not less than ten dollars (\$10.00) nor more than one hundred dollars (\$100.00) for each offense, and shall pay all costs and expenses involved in the conviction. Each day the violation continues shall constitute a separate offense.

ARTICLE VI

SEVERABILITY, AMENDMENT, ADOPTION, AND EFFECTIVE DATE

A. Severability

Should any article, section, subsection, or provision of these subdivision regulations be declared by a court of competent jurisdiction to be invalid or unconstitutional, such decision shall in no way affect the validity or constitutionality of these subdivision regulations as a whole, or any part thereof other than the part so declared to be invalid or unconstitutional.

B. Amendment

Any article, section, subsection, or provision of these subdivision regulations proposed to be amended shall be published as provided by law for the publication of all ordinances. Before an amendment can be adopted, a public hearing as prescribed by law shall be held on the proposed amendment. Following the legal adoption of any amendment, a certified copy of same must be forwarded by the governing body to the Chancery Clerk of Yazoo County, Mississippi.

C. Adoption

Before adoption of these subdivision regulations, a public hearing was held on _____ as required by law, at which time an opportunity was afforded all interested parties to express their opinions and/or objections.

D. Effective Date

These subdivision regulations shall be in full force and effect thirty (30) days after their formal adoption by the Yazoo City Mayor and Board of Aldermen.

PASSED AND ADOPTED, first as to sections, then as a whole, at the _____ meeting of the Mayor and Board of Aldermen of the City of Yazoo City, Mississippi, by the following vote:

Voting Aye _____

Voting Nay _____.

APPROVED:

_____ day of _____, 19____.
(Mayor)

ATTEST:

_____ day of _____, 19____.
(City Clerk)

APPENDICES

APPENDIX A
LEGAL CERTIFICATES

FORM 1

OWNER'S CERTIFICATE AND DEDICATION

I (We) the undersigned _____
do hereby certify that I (we) am (are) the owner(s) of and the only person(s) having
any right, title, or interest in the land shown on the plat of _____,
and that the plat represents a correct survey of the above described property
made with my (our) consent, and that I (we) hereby dedicate for public use all streets
shown on said plat. The easements shown on the plat are created for the installation
and maintenance of public utilities. I (we) hereby guarantee a clear title to all
lands so dedicated from myself (ourselves) and my (our) heirs or assigns forever,
and have caused the same to be released from all encumbrance so that the title is
clear, except as shown in the abstractor's certificate.

RESTRICTIONS (if any, follow here): _____

Witness my hand, this the _____ day of _____, 19____.

Signature: _____

Attest: _____

Attest: _____

FORM 3

CERTIFICATE OF APPROVAL OF STREETS AND UTILITIES

I hereby certify that all streets, utilities, and other improvements have been installed in an acceptable manner and according to city specifications in the subdivision entitled _____, or that surety in the amount of \$ _____ has been posted with the Planning Commission to assure completion of all required improvements in case of default by the subdivider.

Witness my hand, this the _____ day of _____, 19 _____.

Signature: _____
City Street Superintendent or other approving agent

Attest: _____

FORM 4

CERTIFICATE OF APPROVAL OF ELECTRIC UTILITY EASEMENTS

I hereby certify that all electric power utility easements shown on the subdivision plat entitled _____ are adequate to meet the foreseeable needs of the area subdivided.

Witness my hand, this the _____ day of _____, 19____.

Signature: _____

Local Electric Power
Association Representative

Attest: _____

FORM 5

CERTIFICATE OF THE APPROVAL OF THE WATER AND GAS SYSTEMS

I hereby certify that the water and gas supply systems installed or proposed for installation in the subdivision plat entitled _____ fully meet the requirements of the State Board of Health and the Utility Commission of the City of Yazoo City, Mississippi, and are hereby approved as shown.

Witness my hand, this the _____ day of _____, 19____.

Signature: _____

Approving Authority

Attest: _____

FORM 6

CERTIFICATE OF THE APPROVAL OF THE SEWER SYSTEM

I hereby certify that the sewer system installed or proposed for installation in the subdivision plat entitled _____ will comply with the requirements of the State Air and Water Pollution Control Commission and the State Board of Health, and will be approved as shown.

Witness my hand, this the _____ day of _____, 19____.

Signature: _____

Health Officer

Attest: _____

FORM 7

CERTIFICATE OF APPROVAL FOR RECORDING

I hereby certify that the plat of _____
has been found to comply with the subdivision regulations for Yazoo City, Missis-
sippi, with the exception of such variances, if any, as are noted in the minutes
of the Planning Commission, and that it has been approved for recording in the
office of the Chancery Clerk of Yazoo County.

Witness my hand, this the _____ day of _____, 19____.

Signature: _____
Secretary, Planning Commission

Attest: _____

FORM 8

CERTIFICATE OF COMPARISON

We, _____, Chancery Clerk, and _____,
(Civil Engineer) (Land Surveyor), do hereby certify that we have carefully compared
this plat with the original thereof made by the said _____,
(Civil Engineer) (Land Surveyor), and find it to be a true and correct copy of said
map or plat.

Given under my hand and seal of office on this the _____ day of _____,
19____.

Signature: _____
Civil Engineer or Land Surveyor

Attest: _____

FORM 9

CERTIFICATE OF COMPLIANCE

I hereby certify that the monuments and markers shown hereon are in place on the ground, and the plan and plat shown and described hereon is a true and correct representation of a survey to the accuracy designated in the subdivision regulations for the City of Yazoo City.

Witness my signature on this the _____ day of _____, 19____.

Signature: _____

Registered Engineer or Surveyor

Attest: _____

FORM 10

ACKNOWLEDGMENT OF SIGNATURES

Personally appeared before me, the undersigned Notary Public in and for the State and County aforesaid, the within named Owner and (Engineer) (Land Surveyor) with whom I am personally acquainted, and each of whom acknowledges that he executed the foregoing and attached instrument for the purpose therein contained.

Witness my hand and notarial seal of office in Yazoo County, Mississippi, on this the _____ day of _____, 19____.

My commission expires _____, 19____.

Signature: _____

Notary Public

Attest: _____

FORM 11

MORTGAGE RELEASE

In consideration of the platting of the property shown on the plat of _____
_____ and other good and valuable considerations, receipt of which
is hereby acknowledged, I _____ do hereby release,
relinquish, and forever discharge a certain mortgage made by _____
and dated the _____ day of _____, 19____, to _____
which is recorded in Book _____ of Mortgages at Page _____ of the records
of Yazoo County, State of Mississippi, insofar as the same covers all property
dedicated for streets, alleys, parks, boulevards, easements, or other public use,
as shown on said map.

Witness my hand this _____ day of _____, 19____.

Signature: _____
Mortgage Holder

Attest: _____

FORM 12

MAYOR AND BOARD OF ALDERMEN ACCEPTANCE OF PUBLIC DEDICATIONS

Be it resolved by the Mayor and Board of Aldermen of the City of Yazoo City, Mississippi, that the dedications shown on the attached plat of _____ are hereby accepted.

Adopted by the Mayor and Board of Aldermen of the City of Yazoo City, Mississippi, this _____ day of _____, 19____.

Signature: _____
Mayor

Attest: _____

FORM 13

CERTIFICATE OF CITY CLERK

I, _____, City Clerk of the City
of Yazoo City, State of Mississippi, do hereby certify that I have examined the
records of the city, and find that all deferred payments or unmatured installments
of special assessments have been paid in full, and that there is no special assessment
procedure now pending against the land shown on the plat of _____
_____.

Dated this _____ day of _____, 19____.

Signature: _____

City Clerk

Attest: _____

FORM 14

OFFICIAL RECORDATION CERTIFICATE

I, _____, Chancery Clerk and Clerk of the
Chancery Court in and for said County and State, do hereby certify that this plat
of _____ Subdivision was filed for record in my office
on this the _____ day of _____, 19____, and was duly recorded in
Plat Book _____ at Page _____ of the record of maps and plats of land
of this County.

Given under my hand and seal of office on this the _____ day of _____,
19____.

Signature: _____
Chancery Clerk

Attest: _____

APPENDIX B
Performance Bond

PERFORMANCE BOND

KNOWN ALL MEN BY THESE PRESENTS:

That we, _____, as the Principal and the undersigned Surety, are held and firmly bound unto the City of Yazoo City, Mississippi hereafter called City, in the full sum of _____ DOLLARS (\$_____), for the payment of which, well and truly to be made, we, and each of us, bind ourselves jointly and severally, by these presents.

Dated this _____ day of _____, A.D., 19_____.

The conditions of this obligation are such that,

WHEREAS, PRINCIPAL has submitted to the City a preliminary plat for subdivision of a tract of land described as follows:

AND, WHEREAS, PRINCIPAL has, pursuant to the ordinances of the City of Yazoo City, elected to file this bond in lieu of actual completion of improvements and utilities in the above subdivision.

NOW, THEREFORE, if the PRINCIPAL shall, within one (1) year from the date of approval of the final plat of the subdivision, faithfully install and complete all improvements and utilities in the subdivision according to requirements or ordinances, approved plans and specifications, subdivision rules and regulations of the City, and pay all bills for contractors, subcontractors, labor and materials incurred in completion thereof; and shall hold harmless and indemnify the City and all interested property owners against liability, loss or damage by reason of failure of PRINCIPAL to faithfully perform the conditions hereof, then this obligation shall be null and void, otherwise to remain in full force and effect;

PROVIDED, however, that actions upon this bond by contractors, subcontractors, laborers or materialmen shall be limited to six months from and after completion of the improvements and utilities above referred to.

Signed, sealed and delivered the day and year first above written.

Principal

_____ day of _____, 19_____.

ATTEST:

Secretary

_____, 19____

_____, 19____

ATTEST:

Secretary

Approved as to form and legality this _____ day of _____, 19____.

Attorney

Approved by the Mayor and Board of Aldermen of the City of Yazoo City, Mississippi,
this _____ day of _____, 19____.

ATTEST:

Clerk

Mayor

APPENDIX C
Cement Concrete Curbs and Gutters

Description

This item shall consist of Portland cement concrete curbs and gutters, constructed on a prepared subgrade in accordance with these specifications and the cross-section shown.

Materials

1. Aggregates

Coarse aggregate shall consist of crushed stone or gravel. It shall consist of clean, hard, tough, durable pieces, free from injurious amounts of soft, friable, thin, elongated, or laminated pieces, soluble salts, or organic or other deleterious matter.

Fine aggregate shall consist of either a natural sand or stone sand composed of sound particles of approved stone. All sand shall be free of clay or other adherent coatings and injurious amounts of deleterious matter.

2. Cement

Standard Portland cement and high early strength Portland cement shall meet the requirements of current Federal specifications SS-C-192 or its equivalent.

3. Water

Water shall be free from oil, acids, alkali, and/or vegetable matter, and shall be clean.

4. Gradation

When tested by means of laboratory sieves the aggregate shall meet the following requirements:

<u>Sieve Designation</u>	<u>Percent by Weight</u> <u>Passing Square Mesh Sieve</u>	
	<u>Coarse Aggregate</u>	<u>Fine Aggregate</u>
1 1/2 inch	100	-
1 inch	95 - 100	-
1/2 inch	35 - 70	-
3/8 inch	-	100

Percent by Weight
Passing Square Mesh Sieve

<u>Sieve Designation</u>	<u>Coarse Aggregate</u>	<u>Fine Aggregate</u>
No. 4	0 - 10	95 - 100
No. 16	-	45 - 80
No. 50	-	10 - 30
No. 100	-	2 - 10

5. Proportions

The concrete shall contain six (6) bags of cement per cubic yard, and shall be proportioned as follows:

<u>Per Bag of Cement</u>		
<u>Quantity of Water</u>	<u>Coarse Aggregate</u>	<u>Fine Aggregate</u>
6 gals.	345 lbs.	188 lbs.
(Approximate 1:2:4 mix.)		

Note: Weight of aggregates is based on a specific gravity of two and sixty-five hundredths (2.65). Compressive strength shall be three thousand (3,000) pounds per square inch at two (2) days.

Construction Methods

1. Preparation of Subgrade

All boulders, organic material, soft clay, spongy material, and any other objectionable material shall be removed and replaced with approved material. The subgrade shall be properly shaped, rolled, and uniformly compacted to conform with the accepted cross-sections and grades.

2. Forms for Concrete

The forms for the concrete shall be of wood or metal, straight, free from warps or kinks, and of sufficient strength. They shall be staked securely enough to resist the pressure of the concrete without spring. When ready for the concrete to be deposited, they shall not vary from the approved line and grade, and shall be kept so until the concrete has set. Where a face form for a curb is used, it shall be so designed as to permit it to be securely fastened to the other forms.

3. Placing and Finishing Concrete

Just prior to placing the concrete, the subgrade shall be moistened. Mixed to the proper consistency, the concrete shall be placed in the forms and

thoroughly tamped in place so that all honeycombs are eliminated and sufficient mortar is brought to the surface. Next, the surface shall be brought to a smooth even finish by means of a wooden float, and all faces adjacent to the forms shall be spaded, so that after the forms are stripped the surface of the faces will be smooth, even, and free of honeycomb. All edges shall be tool rounded.

4. Expansion and Contraction Joints for Concrete

Half-inch (1/2") expansion joints shall be placed at intervals not exceeding sixty (60) feet. At intervals not greater than ten (10) feet nor less than five (5) feet, the concrete shall be scored for a depth equal to one-third (1/3) the total depth of the concrete.

5. Curing Concrete

After being laid, the concrete shall be kept moist for a period of not less than seven (7) days (longer if necessary), and shall be protected from the elements in a satisfactory manner.

6. Backfilling

Backfill shall consist of suitable selected material, and shall be placed and tamped in layers of not over six (6) inches in depth until firm and solid. Backfilling shall be done immediately after the concrete forms are removed.

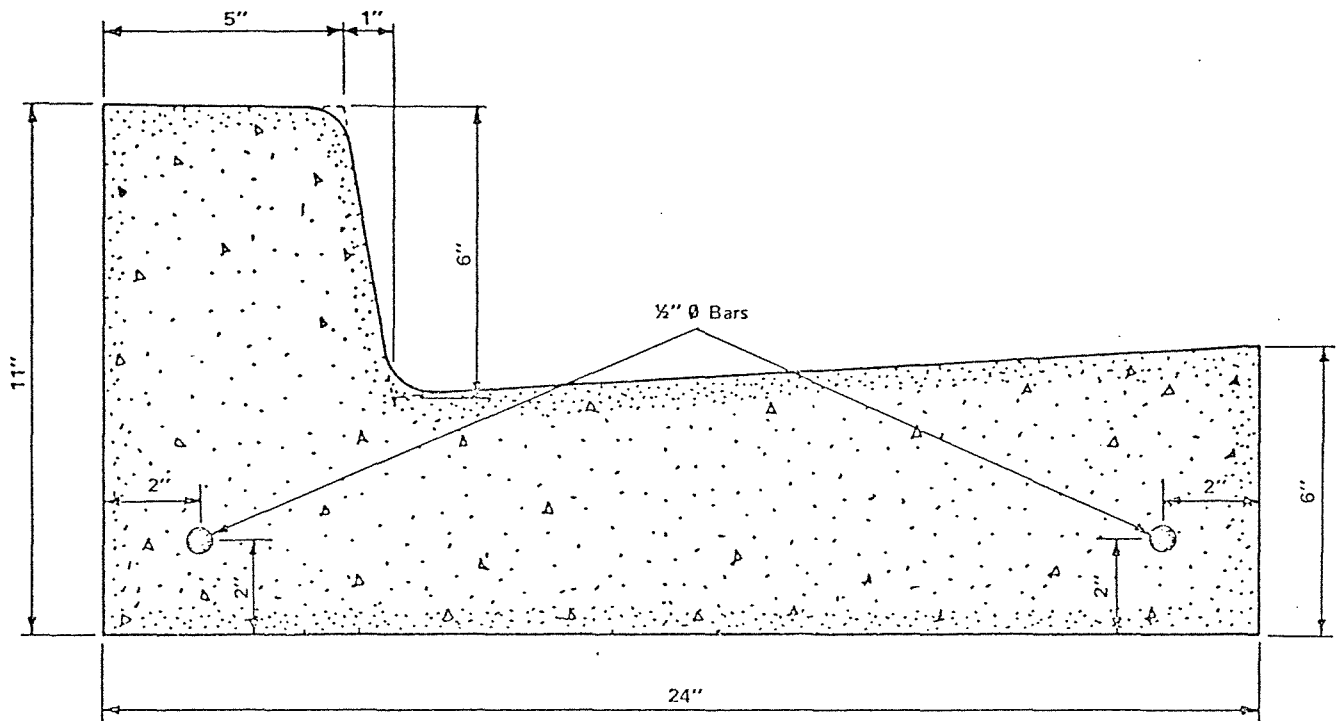
Seasonal Limits

1. No concrete shall be poured on a frozen or thawing subgrade, during unfavorable weather conditions, or when the temperature is 38°F and falling.

2. Cross Section

The cross section shall be as shown below.

Appendix Figure C-1
CEMENT CONCRETE CURB AND GUTTER



APPENDIX D
WEARING SURFACE CONSTRUCTION STANDARDS

ONE COURSE ASPHALTIC CONCRETE WEARING SURFACE (PLANT MIXED)

Description

This wearing surface shall consist of aggregate and bituminous material mixed in a central plant, constructed on the prepared base in accordance with these specifications, and in conformity with the lies, grades, and typical cross sections as shown on the accepted drawings. The compacted thickness of the wearing surface shall be not less than two and one-half (2 1/2) inches.

Materials

1. Liquid asphalt for the tack coat shall be grade RC-2.
2. Asphalt cement to be used in the asphaltic concrete shall be uniform in character, free from water, shall not foam when heated to 350°F., and shall meet the following requirements for penetration:
Penetration at 77°F., 100 grams, 5 seconds - 100 to 120.
No mineral matter other than that naturally contained in the asphalt shall be present.
3. The aggregate shall consist of fine gravel and sand, disintegrated granite, or other similar granular materials. The portion of the material retained on a No. 4 sieve shall be known as filler.
4. The materials shall be graded and proportioned as follows:

<u>Sieve Designation</u>	<u>Percent by Weight Passing Square Mesh Sieve</u>
1 inch	100
3/4 inch	85 - 100
No. 4	60 - 95
No. 10	45 - 80
No. 200	5 - 15
Asphalt by weight	4 - 8

Construction Method

1. Applying Tack Coat

Immediately before placing the bituminous mixture, the existing surface shall be cleaned of loose or deleterious material by sweeping with a power broom or hand broom. The surface shall be thoroughly dry before applying the tack coat. When the existing surface has been put in proper condition,

a tack coat shall be applied to the surface at the rate of one-tenth (0.1) gallon per square yard by a power distributor of approved type, having a pressure of not less than forty (40) pounds per square inch. The bituminous mixture shall be spread and finished immediately after the tack coat has been applied.

2. Spreading and Finishing

Where cement, concrete, or masonry edging is not specified on the accepted plans, suitable side forms of wood or steel shall be firmly fastened in place, and shall be true to line and grade as shown on the accepted plans. These forms shall remain in place until initial compaction has been obtained.

The bituminous mixture shall be delivered on the job at a temperature of not less than 250°F. Whenever practical, the mixture shall be spread by means of an approved mechanical self-powered paver, capable of spreading the mixture true to the line, grade, and crown as shown on the accepted plans and cross sections.

When the mixture is to be spread by hand, it shall be deposited outside of the area on which it is to be spread. Immediately after it is deposited, it shall be distributed into place by means of hot shovels, and spread with hot rakes in a loose layer of uniform density and correct depth. Loads shall not be dumped and distributed any faster than they can be properly handled by the shovelers and rakers. The raking shall be carefully and skillfully done in such a manner that after the first passage of the roller over the raked mixture a minimum amount of back patching will be required.

Placing of the mixture shall be as continuous as possible, and the roller shall pass over the unprotected edge of the freshly laid mixture only when the laying of the course is to be discontinued for such length of time as to permit the mixture to become chilled.

Longitudinal and transverse joints shall be well bonded and sealed. If necessary to obtain this result, the joints shall be cut back to the full depth of the previously laid course, painted with hot asphalt, and heated. Before placing the mixture against them, all contact surfaces of curbs,

gutters, headers, manholes, etc., shall be painted with a thin uniform coating of hot asphalt cement or asphalt cement dissolved in naphtha. After spreading, the mixture shall be thoroughly compacted by a three-wheel, power-driven roller, weighing not less than ten (10) tons as soon after being spread as it will bear the roller without undue displacement. Rolling shall start longitudinally at the sides, and proceed toward the center width of the rear wheel. The pavement shall then be rolled diagonally in two directions with a tandem roller weighing not less than ten (10) tons and the second diagonal rolling shall cross the lines of the first. Along curbs, headers, manholes, and similar structures, and at all places not accessible to the roller, thorough compaction must be secured by means of hot tampers, and at all contacts of this character the joints between these structures and the surface mixture must be effectively sealed.

3. Seasonal Limits

No asphalt material shall be laid when the temperature of the air is 50°F and falling, or during unfavorable weather conditions.

GRAVEL BASE (DENSE GRADED)

Description

This item shall consist of a base course composed of gravel and filler, constructed on a prepared subgrade in accordance with these specifications and in conformity with the lines and grades as shown on the accepted drawings and in accordance with the typical cross sections as shown on the accepted drawings.

Materials

All materials shall be secured from approved sources. Bank gravel shall consist of hard durable particles or fragments of stone or gravel and a filler of sand or other finely divided mineral matter. The material shall be free from organic matter and lumps or balls of clay and shall be graded as follows:

<u>Sieve Designation</u>	Percent by Weight
	<u>Passing Square Mesh Sieve</u>
1 inch	100
3/4 inch	80 - 100
3/8 inch	50 - 90

	Percent by Weight
<u>Sieve Designation</u>	<u>Passing Square Mesh Sieve</u>
No. 4	40 - 75
No. 10	30 - 55
No. 40	20 - 35
No. 200	8 - 15

Construction Methods

1. Preparation of Subgrade

All boulders, organic material, soft clay spongy material and any other objectionable material shall be removed and replaced with approved material. The subgrade shall be properly shaped, rolled and uniformly compacted to conform with the accepted cross section and grades.

2. Placing, Mixing, and Rolling Base Material

All base course material shall be deposited and spread by means of spreader boxes, or approved mechanical equipment, or from moving vehicles equipped to distribute material in a uniform layer. Each layer shall be not more than three (3) inches in thickness after compaction. Each layer shall be constructed as follows:

After the base course material has been spread it shall be thoroughly blade mixed to the full depth of the layer by alternately blading the entire layer to the center and back to the edges of the road. The material shall be watered during the mixing when needed. When uniform the mixture shall again be spread smoothly to the cross section as shown on the accepted drawing. Immediately following final spreading and smoothing all material placed shall be compacted to the full width by rolling with a smooth wheel power roller weighing eight (8) to ten (10) tons.

Rolling shall progress gradually from the sides to the center, parallel with the center line of the road and lapping uniformly each preceding track by one-half ($\frac{1}{2}$) the width of such track and shall continue until all the surface has been rolled and satisfactory compaction obtained. Any irregularities or depressions that

develop under such rolling shall be corrected by loosening the material at these places and adding or removing material until the surface is smooth and uniform. Blading and rolling shall be performed alternately as required to maintain a uniformly compacted base until a surface or treatment has been applied to the base. Along curbs, header and walls and at all places not accessible to the roller, the base course material shall be tamped thoroughly with mechanical tampers or with approved hand tampers.

3. Seasonal Limits

No base material shall be deposited or shaped when the subgrade is frozen or thawing or during unfavorable weather conditions.

SAND CLAY BASE

Description

This item shall consist of a base course composed of sand clay mixtures constructed on a prepared subgrade in accordance with these specifications and in conformity with the lines, grades and typical cross sections as shown on the accepted drawings.

Materials

All materials shall be secured from approved sources.

1. Sand clay shall consist of natural or artificial mixtures of clay or soil binder and gravel, sand or other aggregates. The material shall be free from organic matter and shall be graded as follows:

<u>Sieve Designation</u>	<u>Percent by Weight Passing Square Mesh Sieve</u>
1 inch	100
No. 10	65 - 100

Soil mortar shall be that portion passing the No. 10 sieve and shall be graded as follows:

<u>Sieve Designation</u>	<u>Percent by Weight Passing Square Mesh Sieve</u>
No. 10	100
No. 20	55 - 90
No. 40	35 - 70
No. 200	8 - 25

Construction Methods

1. Preparation of Subgrade

All boulders, organic material, soft clay, spongy material and any other objectionable material shall be removed and replaced with approved material. The subgrade shall be properly shaped, rolled, and uniformly compacted to conform with the accepted cross section and grades.

2. Placing, Mixing, and Rolling Base Material

All base course material shall be deposited and spread by means of spreader boxes, or approved mechanical equipment, or from moving vehicles equipped

to distribute the material in a uniform layer. Each layer shall be not more than three (3) inches in thickness after compaction. Each layer shall be constructed as follows:

After the base course material has been spread it shall be thoroughly blade mixed to the full depth of the layer by alternately blading the entire layer to the center and back to the edges of the road. The material shall be watered during the mixing when needed. When uniform the mixture shall again be spread smoothly to the cross section as shown on the accepted drawing. Immediately following final spreading and smoothing all material placed shall be compacted to the full width by rolling with a smooth wheel power roller weighing eight (8) to ten (10) tons. Rolling shall progress gradually from the sides to the center, parallel with the center line of the road and lapping uniformly each preceding track by one-half the width of such tract and shall continue until all the surface has been rolled and satisfactory compaction obtained. Any irregularities or depressions that develop under such rolling shall be corrected by loosening the material at these places and adding or removing material until the surface is smooth and uniform. Blading and rolling shall be performed alternately as required to maintain a uniformly compacted base until a surface or treatment has been applied to the base. Along curbs, header and walls and at all places not accessible to the roller, the base course material shall be tamped thoroughly with mechanical tampers or with approved hand tampers.

3. Seasonal Limits

No base material shall be deposited or shaped when the subgrade is frozen or thawing or during unfavorable weather conditions.

SOIL CEMENT BASE

Description

This wearing surface shall consist of a base course composed of a combination of soil and Portland cement uniformly mixed, moistened and compacted in accordance with these specifications and in conformity with the lines, grades and typical cross sections as shown on the accepted drawings.

Materials

All materials shall be secured from approved sources.

1. Water shall be free from oil, acids, alkali and vegetable matter and shall be reasonably clean. Sea water shall not be used.
2. Soil for this base course shall consist of the soil found in place and any additional soil that may be required shall be treated as outlined below.

Construction Methods

1. Preparation of Existing Roadway

The existing roadway shall be graded and shaped to conform to the lines, grade and cross sections required for the completed roadway. Materials to be processed shall be distributed so as to provide, after scarifying or pulverizing, a mixture which will comply with the requirements as specified. All unsuitable material shall be removed and replaced with suitable material.

2. Preparation and Mixing

Prior to the application of cement, the soil to be treated shall be scarified and pulverized for a sufficient depth and width to give the compacted cross section shown on the plans. Pulverizing shall continue until eighty (80) percent of the soil will pass a No. 4 sieve. If the soil is wet it shall be manipulated until the percentage of moisture does not exceed the optimum moisture content by more than 2. If the soil is excessively dry, necessary water shall be added.

After the pulverized soil has been shaped to the approximate cross section shown on the plans the specified quantity of Portland cement required for

the full depth of the treatment shall be uniformly spread over the surface in one operation by hand raking or mechanical spreaders. The exact quantity of cement to be used for any specific project can only be determined after an analysis of the soil to be used on the project has been made. Generally ten (10) percent to fourteen (14) percent of cement by volume will be adequate.

After the cement has been distributed it shall be mixed with the loose soil for the full depth of the treatment by means of disc harrows or other approved methods. Care shall be exercised to see that no cement is mixed below the specified depth. Mixing shall continue and be repeated as may be necessary to insure a thoroughly uniform intimate mix of the soil and cement and until the resultant mixture is homogeneous and uniform in appearance.

When the cement and soil have been brought to a uniform mix the required quantity of water shall be added. Water shall be added in amounts and in such a manner that no puddles or excessive amounts of water will accumulate in any one place. Each application of water shall be partially incorporated with the soil and cement by the use of field cultivators, disc harrows or other mixing equipment. The mixing equipment shall be set to cut the full depth of the pavement being processed. When this operation is completed, the percentage of moisture in the mixture shall not vary from the optimum percentage by more than one-tenth (1/10). The determination of the specified optimum moisture shall be made in the field by a moisture density test, on representative samples of soil cement mixture obtained from the roadway toward the conclusion of the damp mix.

3. Compaction

Prior to the beginning of compaction, the mixture shall be thoroughly loosened for its full depth. Compaction with a sheep's foot roller shall begin at the bottom and continue until not over one (1) inch depth of the loose soil cement mixture remains. The materials shall be uniformly compacted to the specified density as determined by the field moisture density test. After the mixture is compacted, the surface of the roadway shall be shaped to the required line grades and cross sections and shall then be lightly scarified to loosen any imprints left by the sheep's foot

roller or other equipment. The resulting surface shall then be rolled with a smooth wheel roller. Rolling and broom dragging shall be done in such a manner as to produce a smooth, closely knit surface free from cracks, ridges or loose material. During the rolling process and after the rolling has been completed the surface shall be checked with a ten (10) foot straight edge and any irregularities greater than one-half ($\frac{1}{2}$) inch in ten (10) feet shall be corrected by loosening the surface and reshaping and compacting it to the proper lines, grades and cross section..

4. Curing

After the roadway has been finished it shall be protected against rapid drying for a period of seven (7) days, by applying a two (2) inch covering of suitable earth or other approved materials. The covering shall be moistened as soon as applied and subsequently as may be necessary.

5. Alternate Method of Construction

If a traveling mixing plant is used it shall be capable of mixing cement, soil and water and depositing it on the subgrade in a satisfactory manner. Compaction of the mixed soil cement shall begin not later than one-half ($\frac{1}{2}$) hour after its final mixing.

6. Construction Limitations

Cement shall not be applied to a larger area than can be completed in one day's operation. Compaction shall be completed not later than six (6) hours after the beginning of the application of water. No cement shall be applied when the air temperature is less than 40°F in the shade and falling.

7. Opening to Traffic

Traffic shall be excluded from the completed roadway for a period of at least seven (7) days.

ASPHALT STABILIZED BASE

Description

This item shall consist of a base course composed of a combination of soil and asphalt uniformly mixed and compacted in accordance with these specifications and in conformity with the lines, grades, and typical cross sections as shown on the accepted drawings.

Materials

All materials shall be secured from approved sources.

1. Soil for this base course shall consist of the material in the road bed or of approved selected material.
2. Asphalt used shall be an approved cut back asphalt. Type and quantity may be determined locally.

Construction Methods

1. Preparation of Existing Roadway

The existing road bed material shall be plowed to a depth approximately equal to that of the finished stabilized road bed and for the full width of the proposed finished surface. After plowing, the road bed shall be harrowed, and all roots, foreign matter, and all stone or pieces of rock larger than two (2) inches shall be removed and disposed of and replaced with suitable material. The road bed shall then be bladed until uniform lines and grades have been obtained.

2. Moisture Content of Soil

Mixing operations shall not be performed when the moisture content of the soil is such as to prevent a uniform and intimate mixture of the cut back asphalt and soil.

3. Applying and Mixing Stabilizing Material

After the road bed material has been adjusted to the proper moisture content and shaped to the desired line and grade the cut back asphalt shall be applied uniformly over the entire road bed by means of a pressure distributor. The asphalt shall be applied in a series of applications, with satisfactory mixing operations between applications. The usual.

amount of asphalt applied in each application is approximately fifty-hundredths (0.50) gallon per square yard but this amount will be subject to change as determined by an analysis of the soil and/or other local conditions.

Mixing shall be done with blade graders, disc harrows or by other approved methods. Continuous mixing shall follow immediately after each application of the asphalt and the mixing shall continue until the soil and asphalt are thoroughly mixed and is homogeneous and uniform in appearance.

4. Compaction

Prior to compaction, the mixture shall be aerated by loosening and turning the mixture by means of plows, harrows, blades and/or other satisfactory equipment until the volatile matter in the asphalt has evaporated to such a degree that the materials begin to become "tacky."

After the mixture has set properly it shall be compacted by rolling with a sheep's foot roller. Rolling shall begin at the bottom and continue until not more than one (1) inch depth of the loose mixture remains. The rolling shall continue until the mixture is compacted to the desired uniform density. After the mixture is compacted the surface of the road bed shall be shaped to the required line, grade and cross section and shall then be lightly scarified to loosen any imprints left by the sheep's foot roller or other equipment. The resulting surface shall then be rolled with a smooth wheel tandem roller weighing from five (5) to eight (8) tons. Rolling with the tandem roller shall be done longitudinally, beginning at the edges and proceeding toward the center. The rolling shall be continued until uniform compaction and smoothness have been obtained.

5. Surface Requirements

The finished surface of the pavement shall be checked with a templet cut to the required crown and cross section, and with a ten (10) foot straight edge laid parallel to the center line of the road. All irregularities greater than one quarter ($\frac{1}{4}$) inch shall be corrected by scarifying and removing pre-mixed bituminous material as may be required, after which the entire area shall be rolled and brought to a satisfactory state of

compaction. As soon as practicable after the construction of the pavement has been completed and before any traffic is permitted a bituminous prime or tack coat shall be applied. After the prime or tack coat has been applied, all traffic shall be kept off the road until the bituminous material has penetrated and dried out so that it does not pick up under traffic. If desired a minimum cover coat of stone or gravel chips may be applied to the prime or tack coat to prevent its picking up under traffic.

6. Seasonal Limits

The bituminous material shall not be applied when the temperature of the air is less than 40°F in the shade, or when the weather or road bed material conditions are unsuitable.

EMULSIFIED ASPHALT STABILIZED BASE

Description

This item shall consist of a base course composed of a combination of aggregate and emulsified asphalt uniformly mixed and compacted in accordance with these specifications and in conformity with the lines, grades, and typical cross sections as shown on the accepted drawings.

Materials

All materials shall be secured from approved sources.

1. Soil for this base course shall consist of the material in the road bed or of approved selected material.
2. Asphalt used shall be emulsified asphalt of a stable mixing grade conforming to the Federal Specification SSA674a - Type VI.

Construction Methods

1. Preparation of Existing Roadway

The existing road bed material shall be plowed to a depth approximately equal to that of the finished stabilized road bed and for the full width of the proposed finished surface. After plowing, the road bed shall be harrowed, and all roots, foreign matter, and all stone or pieces of rock larger than two (2) inches shall be removed and disposed of and replaced with suitable material. The road bed shall then be bladed until uniform lines and grades have been obtained.

2. Moisture Content of Soil

Mixing operations shall not be performed when the moisture content of the soil is such as to prevent a uniform and intimate mixture of the emulsified asphalt and soil.

3. Applying and Mixing Stabilizing Material

After the road bed material has been adjusted to the proper moisture content and shaped to the desired line and grade the emulsified asphalt shall be applied uniformly over the entire road bed by means of a pressure distributor. The emulsified asphalt shall be applied in a series of applications, with satisfactory mixing operations between applications. The usual amount of emulsified asphalt applied in each application is approximately fifty-hundredths (0.50) gallon per square yard, but this amount will be subject to change as determined by an analysis of the soil and/or other local conditions. Mixing shall be done with blade graders, disc harrows or by other approved methods. Continuous mixing shall follow immediately after each application of the asphalt and the mixing shall continue until the soil and asphalt are thoroughly mixed and is homogeneous and uniform in appearance.

4. Compaction

Prior to compaction, the mixture shall be aerated by loosening and turning the mixture by means of plows, harrows, blades and/or other satisfactory equipment until compaction can be readily obtained. After the mixture has set properly it shall be compacted by rolling with a pneumatic-tired roller of approved design until it is thoroughly compacted, dense and tightly bonded and to the desired uniform density. Should depressions occur during the rolling, additional base mixture shall be carefully worked in by blading and rolling. The resulting surface shall then be rolled with a smooth wheel tandem roller weighing from five (5) to eight (8) tons. Rolling with the tandem roller shall be done longitudinally, beginning at the edges and proceeding toward the center. The rolling shall be continued until uniform compaction and smoothness have been obtained.

5. Surface Requirements

The finished surface of the pavement shall be checked with a template cut to the required crown and cross section, and with a ten (10) foot straight edge laid parallel to the center line of the road. All irregularities greater than one quarter ($\frac{1}{4}$) inch shall be corrected by scarifying and removing pre-mixed bituminous material as may be required, after which the entire area shall be rolled and brought to a satisfactory state of compaction. When the stabilized base has dried to the required minimum moisture content, an application of immiscible type emulsified asphalt primer shall be applied at the rate of approximately one-third ($\frac{1}{3}$) gallon per square yard. The primer application shall be protected from traffic wherever possible, until it has penetrated into the stabilized base. The speed of traffic shall be controlled to prevent excessive loss of the material. No surfacing shall be placed over the primer application until it has been allowed to thoroughly penetrate and harden.

6. Seasonal Limits

The bituminous material shall not be applied when the temperature of the air is less than 40°F in the shade, or when the weather conditions or that of the existing road bed materials are unsuitable.

SAND STABILIZED SHELL BASE

Description

This item shall consist of a base course composed of oyster shell, sand and water constructed upon a prepared sub-grade in accordance with these specifications and in conformity with the lines, grades and typical cross-sections as shown on the accepted drawings.

Materials

Sand shell material shall consist of durable particles of oyster or clam shell mixed in a pug mill, at a central mixing plant or traveling plant, with clean, hard, durable sand in such proportions so that when tested by means of square mesh laboratory sieves the blended sand and shell shall meet the following gradation requirements:

	<u>Percent by Weight</u>
Retained on 2 1/2" screen	0 - 8
Retained on 1" screen	12 - 37
Retained on 1/4" screen	50 - 80
Passing 200 mesh sieve	3 - 9

The material passing the 40 mesh sieve shall be known as soil binder and shall meet the following requirements:

1. The liquid limit shall not exceed 35.
2. The plasticity index shall not exceed 15.

Construction Methods

1. Preparation of Subgrade

All boulders, organic material, soft clay spongy material and any other objectionable material shall be removed and replaced with approved material. The sub-grade shall be properly shaped, rolled and uniformly compacted to conform with the accepted cross sections and grades.

2. Placing, Mixing, and Rolling Base Material

All material deposited shall be spread and shaped the same day; except that in the event inclement weather or other unforeseen circumstances render it impractical to spread or shape the material during the first twenty-four (24) hours, it shall then be scarified and mixed until uniform before it is spread.

All material shall be spread by means of spreader boxes, or approved mechanical equipment, or from moving vehicles equipped to distribute the material in uniform layers. Each layer after compaction shall be not more than one-half (1/2) the total compacted thickness of the base and shall be constructed as follows:

After the base course material has been spread it shall be thoroughly blade mixed to the full depth of the layer by alternately blading the entire layer to the center and back to the edge of the road. The material shall be watered during mixing if necessary.

When uniform the mixture shall again be spread smoothly to the cross section as shown on the accepted drawings.

Immediately following final spreading and smoothing all material placed shall be compacted to the full width by means of a sheeps-foot or pneumatic tired roller. Rolling and blading shall continue until maximum density has been obtained and the base course is shaped to conform with the accepted grades and cross sections.

Any irregularities or depressions that develop under such rolling shall be corrected by loosening the material at these places and adding or removing material until the surface is smooth and uniform.

Shaping and rolling shall be performed alternately as required to maintain a uniform compacted base until a wearing surface has been applied to the base.

Along curbs, headers, and walls, and at all places not accessible to the roller, the base course material shall be tamped thoroughly with mechanical tampers or with approved hand tampers.

3. Seasonal Limits

No base course material shall be deposited or shaped when the sub-grade is frozen and thawing or during unfavorable weather conditions.

PROCESSED REEF SHELL BASE

Description

This item shall consist of a base course composed of dead reef shell and shall be constructed on a prepared subgrade in accordance with these specifications and in conformity with the lines, grades and cross sections shown on the accepted drawings.

Materials

All material shall be secured from approved sources.

Dead reef shell shall consist principally of dead oyster shell but may contain dead clam or other dead shell as encountered in the reef. The foreign matter content of the processed shell, as determined by washing, shall not exceed three (3) percent of the dry weight of the shell.

Construction Methods

1. Preparation of Subgrade

All boulders, organic material, soft clay spongy material and any other objectionable material shall be removed and replaced with approved material. The subgrade shall be properly shaped, rolled and uniformly compacted to conform with the accepted cross section and grades.

2. Placing, Mixing and Rolling

All base course material shall be deposited and spread by means of spreader boxes, or approved mechanical equipment, or from moving vehicles equipped to distribute the material in a uniform layer. Each layer shall be not more than three (3) inches after compaction.

Where required, the first layer of shell shall be blended with sand or other material and uniformly mixed and spread.

After each layer of the base course material has been spread, it shall be watered in quantities not to exceed the optimum of moisture that will attain maximum compaction by rolling. Immediately after watering each layer it shall be rolled with a sheeps foot roller in order to break sufficient shell into binder material.

After rolling with the sheeps foot roller, each layer shall be machined and rolled with a smooth wheel power roller, weighing from five (5) to

seven (7) tons, alternately until maximum compaction and true cross section have been obtained.

Any irregularities or depressions that develop under such rolling shall be corrected by loosening the material at these places and adding or removing material until the surface is smooth and uniform.

Along curbs, headers, and walls, and at all places not accessible to the roller, the base course material shall be tamped thoroughly with mechanical tampers or with hand tampers.

Seasonal Limits

The base course material shall not be deposited on a muddy subgrade or during unfavorable weather conditions.

APPENDIX E
CEMENT CONCRETE SIDEWALK

CEMENT CONCRETE SIDEWALK

Description

This item shall consist of sidewalks made of one course Portland cement concrete, constructed in accordance with these specifications and the cross section shown.

Materials

All materials shall be secured from approved sources.

1. Aggregate shall be graded as follows:

<u>Sieve Designation</u>	<u>Percent by Weight</u>	
	<u>Coarse Aggregate</u>	<u>Fine Aggregate</u>
1 1/2 inch	100	-
1 inch	95 - 100	-
1/2 inch	35 - 70	-
3/8 inch	-	100
No. 4	0 - 10	95 - 100
No. 16	-	45 - 80
No. 50	-	10 - 30
No. 100	-	2 - 10

2. Water shall be free from oil, acids, alkali, vegetable matter, and shall be clean.
3. Concrete shall contain six (6) bags of cement per cubic yard, and shall be proportioned as follows:

Quantities Per Bag of Cement (Approximately 1:2:4 mix)

<u>Water</u>	<u>Coarse Aggregate</u>	<u>Fine Aggregate</u>
6 gals.	345 lbs.	188 lbs.

Note: Weight of aggregate based on a specific gravity of 2.65.

4. Ready-mix concrete of transit-mix will be accepted. Compressive strength of concrete shall be three thousand (3,000) pounds per square inch at twenty-eight (28) days.

Construction Methods

1. Preparation of Subgrade

All boulders, organic material, soft clay, spongy material, and any other objectionable material shall be removed and replaced with approved material. The subgrade shall be properly shaped, rolled, and uniformly compacted to conform with the accepted cross sections and grades.

2. Forms for Concrete

The forms for the concrete shall be of wood or metal, and shall be straight, free from warps or kinks, and of sufficient strength. They shall be staked securely enough to resist the pressure of the concrete without spring. When ready for the concrete to be deposited, they shall not vary from the approved line and grade, and shall be kept so until the concrete has set.

3. Placing and Finishing Concrete

Just prior to placing the concrete, the subgrade shall be moistened. Mixed to the proper consistency, the concrete shall be placed in the forms and thoroughly tamped in place so that all honeycombs are eliminated and sufficient mortar is brought to the surface. Next, the surface shall be brought to a smooth even finish by means of a wooden float, and all faces adjacent to the forms shall be spaded so that after the forms are stripped the surface of the faces will be smooth, even, and free of honeycomb. All edges shall be tool rounded. The sidewalks and planting strips shall be sloped one-quarter ($\frac{1}{4}$) inch per foot towards the pavement surface.

4. Expansion Joints and Scoring Concrete

One-half ($\frac{1}{2}$) inch transverse expansion joints shall be placed at intervals not exceeding eight (8) feet. Sidewalks shall be scored to a depth of one (1) inch every four (4) to six (6) feet, and at all intersections with curbs or other structures.

5. Curing Concrete

After being laid, the concrete shall be kept moist for a period of not less than seven (7) days (longer if necessary), and shall be protected from the elements in a satisfactory manner.

6. Backfilling

Backfill shall consist of suitable selected material and shall be placed and tamped until firm and solid. Backfilling shall be done immediately after the concrete forms have been removed.

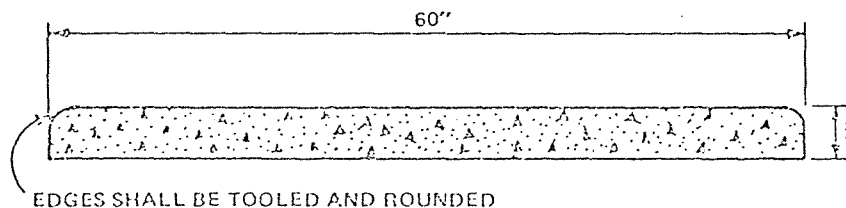
7. Seasonal Limits

No concrete shall be poured on a frozen or thawing subgrade, during unfavorable weather conditions, or when the temperature is 38°F and falling.

8. Cross Section

The cross section shall be as shown below.

Appendix Figure E-1
CEMENT CONCRETE SIDEWALK



The thickness of the sidewalk pictured above shall be increased to six (6) inches when that portion is to be used as a driveway.

APPENDIX F
PRELIMINARY AND FINAL PLAT REVIEW AND APPROVAL FORM

APPENDIX F

PRELIMINARY AND FINAL PLAT REVIEW AND APPROVAL FORM

General Requirements

Name of Subdivision _____

Name of Owner(s) _____

Date Submitted _____ Checked by _____

Number of Copies of Plat Submitted _____

Topographic Map Required? Yes___ No___ Submitted? Yes___ No___

Location of Subdivision _____

Amount of Fee Enclosed \$ _____

Has Plat Been Prepared at the Appropriate Scale? Yes___ No___

Present Zoning Classification of Area _____

		Preliminary		Final	
		Plat		Plat	
		Yes	No	Yes	No
General Platting Requirements					
1.	Is the subdivision laid out to conform with the approved master plan of the area?	_____	_____	_____	_____
2.	Does the street pattern discourage through traffic?	_____	_____	_____	_____
3.	Do the streets intersect at as nearly 90° angles as possible?	_____	_____	_____	_____
4.	Are the number of streets converging at one point kept to a minimum?	_____	_____	_____	_____
5.	Are acute angle intersections kept to a minimum?	_____	_____	_____	_____
6.	Have provisions been made for principal local streets to be continued in adjacent subdivisions without creating hazardous jogs or angles in the thoroughfare pattern?	_____	_____	_____	_____
7.	If cul-de-sacs are shown:				
a.	Do they have adequate turn-around facilities (100 feet diameter minimum)?	_____	_____	_____	_____
b.	Are they not more than 500 feet long?	_____	_____	_____	_____
8.	Excessive long and short blocks are to be avoided. Are blocks a normal size (approximately 1,000 feet in length)?	_____	_____	_____	_____
9.	If a block is excessively long, has a pedestrian crossing been provided in the middle of the block?	_____	_____	_____	_____

	Preliminary		Final	
	Plat		Plat	
	Yes	No	Yes	No
10. Are all existing nonconforming and public uses within the proposed subdivision clearly shown and labeled "nonconforming?"	_____	_____	_____	_____

Required Physical Improvements

1. Has due consideration been given by the subdivider regarding dedication of that portion of land necessary for public uses (school sites, park sites, etc.)?	_____	_____	_____	_____
2. Have all necessary easements for utilities been checked to determine whether they meet the requirements of the utility companies?	_____	_____	_____	_____
3. Have the locations, widths, and other dimensions of proposed streets, alleys, easements, and other open spaces and lots been clearly shown?	_____	_____	_____	_____
4. Are all blocks and lots properly numbered?	_____	_____	_____	_____
5. Do all streets and courts shown on the plat bear tentative names?	_____	_____	_____	_____
6. List of names of streets, courts, or boulevards shown on the plat.				
a. _____	e. _____			
b. _____	f. _____			
c. _____	g. _____			
d. _____	h. _____			

	Preliminary		Final	
	Plat		Plat	
	Yes	No	Yes	No
Do any of the tentative names listed conflict with any existing street names?	_____	_____	_____	_____
7. Is the accurate location and description of all monuments clearly shown? (Permanent monuments of natural stone or concrete should be set to finished grade at such critical points as will enable any skilled surveyor to lay out correctly any lot in the subdivision.)	_____	_____	_____	_____
8. Has the length of lines of all lots; the length and bearing of the lines of all streets, alleys, and easements; the length of all arcs; and the radii, points of curvature, and the tangent bearings in the case of curved lines been checked by the City Engineer?	_____	_____	_____	_____
9. Do all necessary signatures appear on the plat?	_____	_____	_____	_____
10. Is the north point, date, scale, and name of the firm which designed the plat clearly shown?	_____	_____	_____	_____
Zoning Ordinance Requirements				
1. Does the zoning classification of all parcels of land appear on the plat?	_____	_____	_____	_____
2. Are all lots delineated of adequate size to meet the requirements of the appropriate zoning classifications?	_____	_____	_____	_____

Preliminary

Final

Plat

Plat

Yes

No

Yes

No

3. Will a performance bond (to run to the city) be required?

4. Performance bond set at \$ _____

Approval Granted

1. Preliminary Plat:

a. Planning Commission

b. City Council

2. Final Plat:

a. Planning Commission

b. City Council